

# Planning Justification Report

## 1555 18<sup>th</sup> Avenue East

Calloway Real Estate Investment Trust Inc.

City of Owen Sound

Official Plan Amendment

Zoning By-law Amendment

Site Plan Approval

January 2023

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Zoning By-law Amendment  
Site Plan Approval

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**Prepared for:**

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## Contents

1.	Introduction.....	1
1.1	Background and Context .....	1
1.2	Subject Applications .....	1
1.3	Purpose and Scope .....	2
2.	Site Location and Context.....	3
2.1	Site Description .....	3
2.2	Surrounding Context.....	3
2.3	Transportation Network .....	6
3.	Proposed Development .....	9
4.	Policy and Regulatory Context.....	13
4.1	Provincial Policy Statement (2020).....	13
4.2	County of Grey Official Plan .....	16
4.3	City of Owen Sound Official Plan.....	20
4.4	City of Owen Sound Zoning By-law .....	28
5.	Technical Report Summaries.....	31
5.1	Transportation Impact Study and Transportation Plan .....	31
5.2	Functional Servicing and Stormwater Management Report .....	32
5.3	Archaeological Assessment.....	33
5.4	Noise Impact Study .....	33
5.5	Urban Design Brief .....	34
5.6	Tree Inventory and Preservation Plan.....	34
6.	Public Consultation Strategy.....	36
7.	Subject Application and Planning Justification .....	37
7.1	Official Plan Amendment Application .....	37

7.2 Zoning By-law Amendment Application .....37

7.3 Planning Justification.....37

8. Summary and Conclusion.....43

Appendix A - Draft Official Plan Amendment .....44

Appendix B - Draft Zoning By-law Amendment.....50

# 1. Introduction

## 1.1 Background and Context

GSP Group is the planning consultant to Calloway Real Estate Investment Trust Inc (the “Client”) with respect to the vacant, southern area of the property known municipally as 1555 18<sup>th</sup> Avenue East (the “Subject Site”). Our Client is proposing to develop the currently vacant property for multi-family residential (the “Proposed Development”).

A pre-application consultation meeting was held on February 24<sup>th</sup>, 2022, regarding the Subject Application. The provided Issue Summary & Completeness Requirements identify a Planning Justification Report as of complete application requirement. The City also identified an Urban Design Brief as a submission requirement. An Urban Design Brief, which includes an analysis of Section 8 of the City of Owen Sound Official Plan, is submitted as a separate document. In addition to the Planning Justification Report and Urban Design Brief, the following materials have been prepared, based on the pre-application consultation comments:

- Transportation Impact Study and Transportation Plan;
- Functional Servicing and Stormwater Management Brief;
- Archaeological Assessment;
- Noise Impact Study;
- Tree Inventory and Preservation Plan;
- Landscape Plan;
- Draft Official Plan and Zoning By-law amendments; and,
- Grading Plan & Sediment and Erosion Control Plan.

## 1.2 Subject Applications

The Subject Site is designated “Residential” in the City of Owen Sound Official Plan (the “OP”), which permits a range of residential uses including those included in the Proposed Development. The Subject Site is further identified as being “Low Density Residential” in the Sydenham Heights Planning Area. The current zoning is R3 and R3(H) 14.5, which permits single-detached, semi-detached, duplex, townhouse, converted dwellings, and accessory apartments. As such, the Proposed Development requires Zoning By-law Amendment and Official Plan Amendment applications (the “ZBA”, “OPA” and “Subject Applications”). A Site Plan Approval application is also being submitted as part of the application package (“SPA”). The Proposed Development will be implemented through two phases. The SPA application pertains to only Phase 1, as further detailed in Section 3 of this Report.

The site concept submitted for the pre-application consultation meeting demonstrated a redesignation of the noted future Collector Road to a private road. Since this meeting, the site concept has been revised and maintains the future Collector Road.

### **1.3 Purpose and Scope**

This Report has been prepared in support of the Subject Applications and will provide planning justification for the redesignation of the Subject Site, zone change and site-specific provisions.

The objectives of this Report are as follows:

- To provide an overview of the Subject Site, including site description, surrounding uses, and connections to transportation networks;
- To provide an overview of the Proposed Development;
- To provide a summary of existing planning policies and regulations that apply to the Subject Site;
- To provide a summary of the supporting technical studies;
- To provide a summary of the Subject Applications; and,
- To provide a planning justification for the Subject Applications.

## 2. Site Location and Context

### 2.1 Site Description

The Subject Site is located at 1555 18<sup>th</sup> Avenue East and is currently vacant (see **Figure 1**). The Subject Site is legally described as RANGE 5 EGR PT PK LOTS 7 8;16R5510 PT 1 16R7308 PTS 7;TO 14 16R7779 PT PT 7 23 PTS;3 4 5 8 9 13 15 TO 18 20 22. The Subject Site is approximately 6 hectares in area (approximately 5 ha net of the proposed collector road), having a frontage of approximately 415 metres along the proposed collector road. The Subject Site is currently vacant, with a vegetated area on the western portion of the land. There is a fairly significant grade change on the northern side of the Subject Site, sloping downwards toward the existing Wal-mart and Home Depot.

### 2.2 Surrounding Context

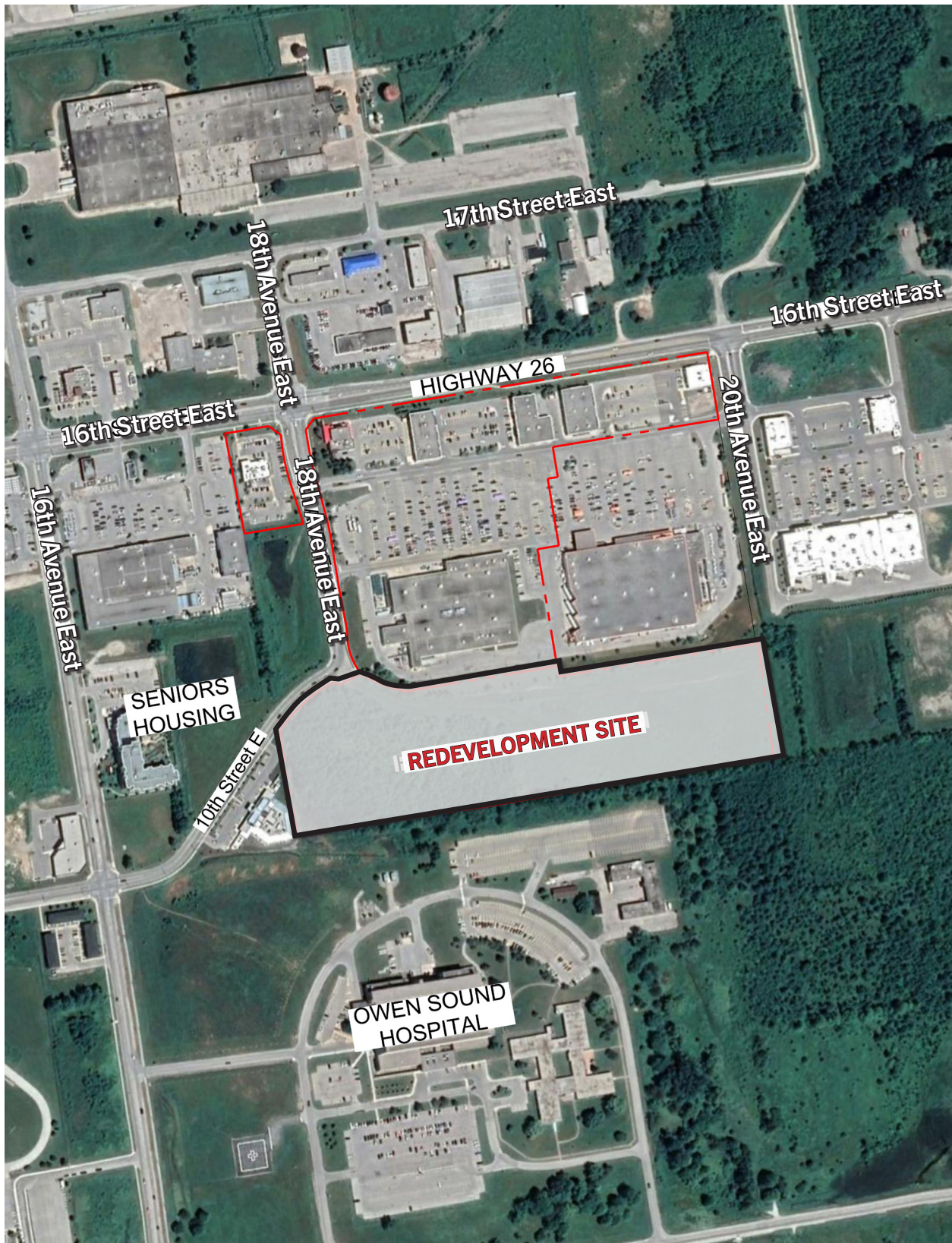
The Subject Site is located in the eastern area of the City of Owen Sound (see **Figure 2**). The surrounding land uses generally include:

North: Immediately adjacent to the north is a significant commercial node in the City. A Wal-mart and Home Depot are directly adjacent, with several retail and commercial uses surrounding.

East: To the east is generally vacant land.

South: Adjacent to the south is the Grey Bruce Health Services (Hospital) Owen Sound location as well as vacant land. The hospital is a significant land use in the surrounding area.

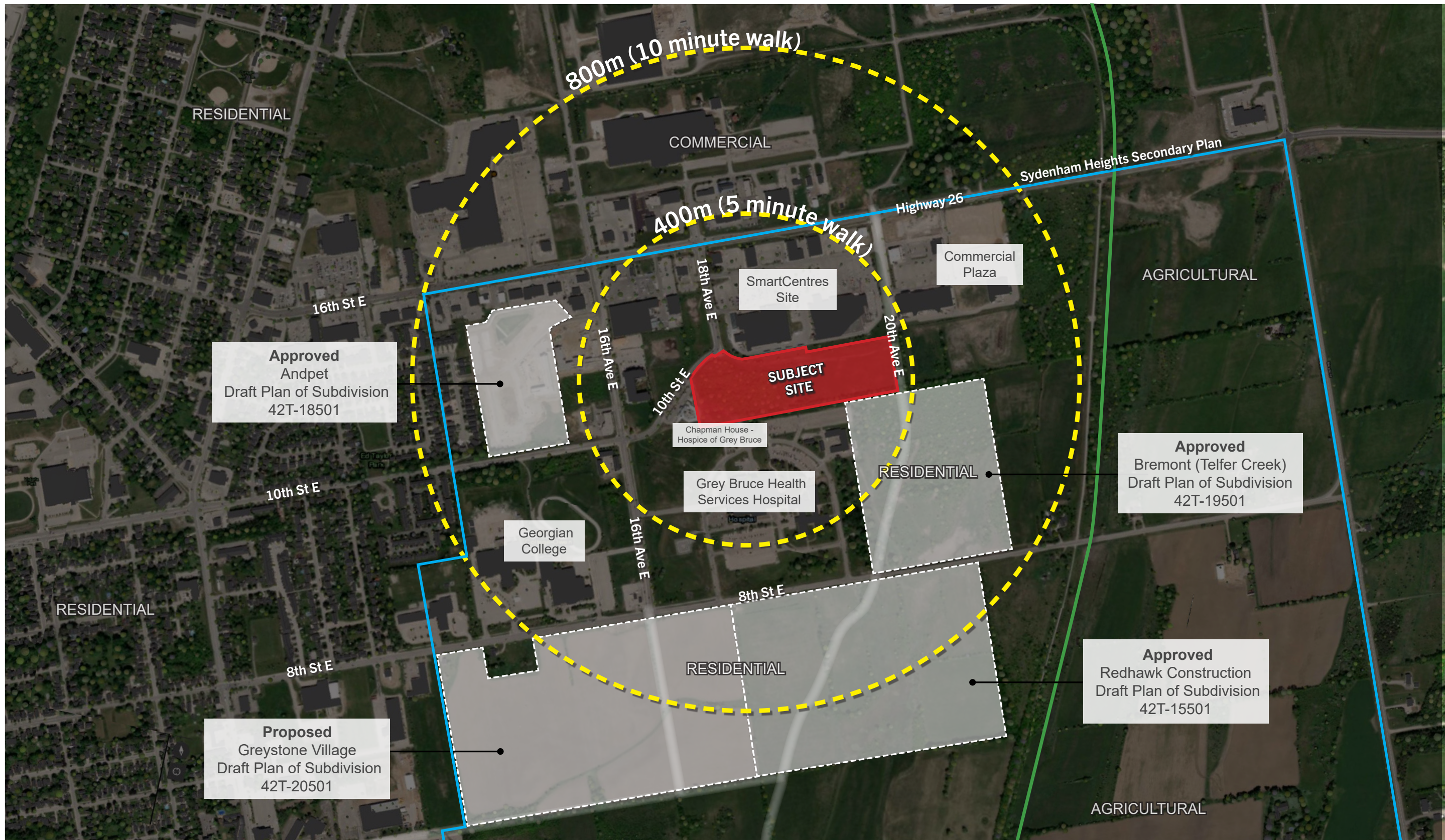
West: Immediately west of the Subject Site is Grey Bruce Hospice, with additional institutional, commercial, and residential uses further to the west of the Subject Site. Downtown Owen Sound is 2.5 kilometres from the Subject Site, directly west along 10<sup>th</sup> Street East.



Site Aerial  
Source: Google Earth (2019)

Figure  
**1**



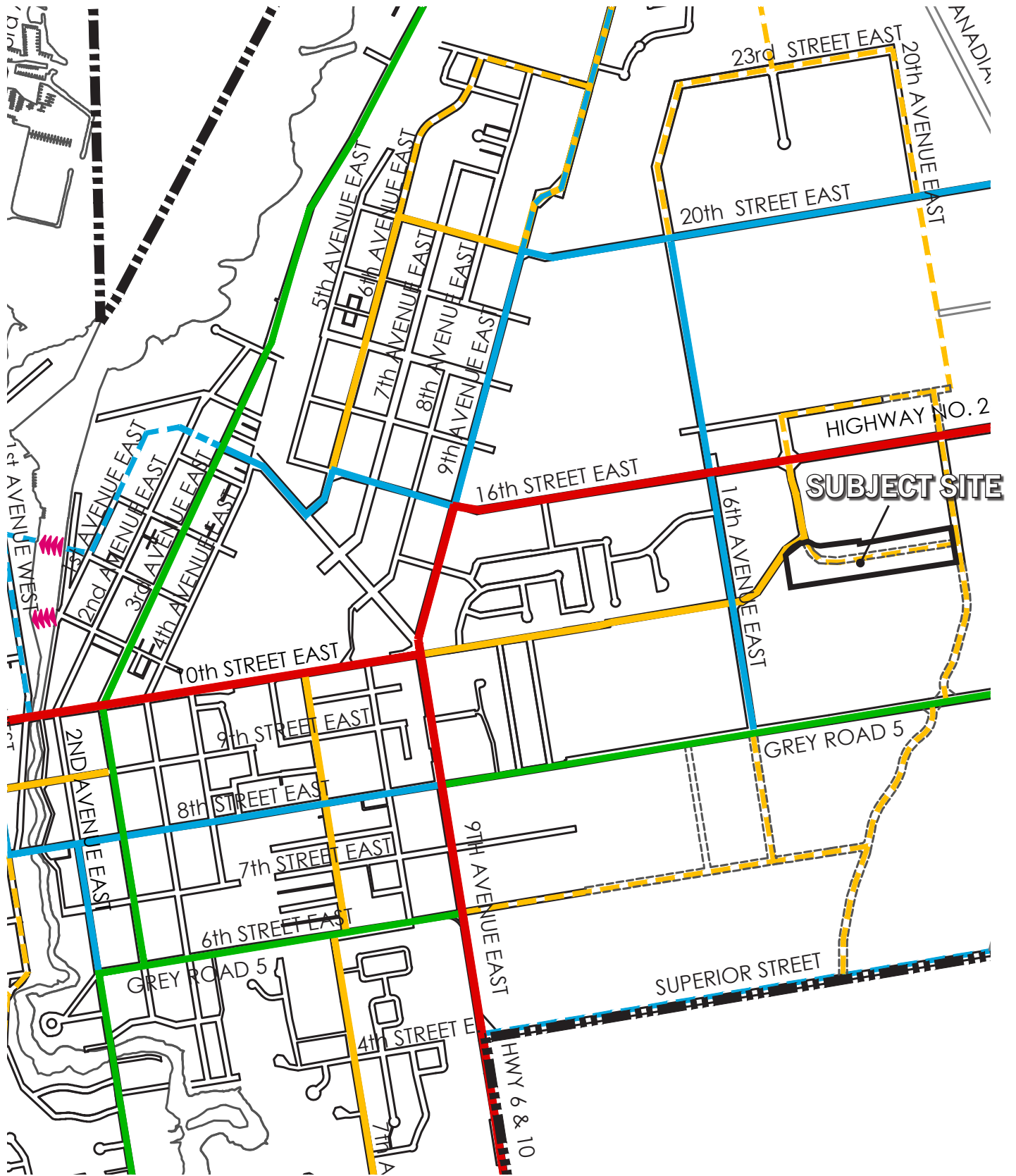


## 2.3 Transportation Network

The Subject Site is located in an area of the City that is connected to transit, recreational trails, and has convenient road access to the greater City (see **Figure 3 and 4**). The Subject Site is located where 18<sup>th</sup> Avenue East meets 10<sup>th</sup> Street East, both of which are classified as Collector Roads in the Official Plan. These roads provide one travel lane in each direction, providing direct access to downtown Owen Sound as well as the greater commercial area surrounding the Subject Site. A Transportation Impact Study (“TIS”) was undertaken by CGH Transportation, analyzing the existing and future traffic impacts related to the Proposed Development. The TIS finds that based on the expected future traffic levels, intersections in the surrounding area are expected to operate with good overall line of sight and delay, including recommended separated eastbound left turn lane at the intersection of 10<sup>th</sup> Street East and 18<sup>th</sup> Avenue East as a mitigation measure.

The Grey County CP Rail Trail is located near the Subject Site. This multi-use trail is 77 kilometres along, beginning near the Harry Lumley Bayshore Community Centre to the south of the Subject Site. A trail connection is located to the east of the Subject Site along 16<sup>th</sup> Street East. The trail follows a wide arc east of the city and runs parallel to Highway 6 to Chatsworth. In addition to this trail, Schedule D of the Owen Sound Official Plan designates 10<sup>th</sup> Street East, 8<sup>th</sup> Street East, and 16<sup>th</sup> Avenue East as bicycle routes.

The Subject Site is in proximity to public transit. The East Bayshore bus route operates along 18<sup>th</sup> Street East and 8<sup>th</sup> Street East, where a stop is located within walking distance of the Subject Site. The bus route provides access to the major commercial centre along 16<sup>th</sup> Street East, hospital, and continues toward the downtown area of Owen Sound and the central transit terminal. The Core bus route operates along 16<sup>th</sup> Avenue East and 10<sup>th</sup> Street East, where a stop is again located within walking distance of the Subject Site.



City of Owen Sound Official Plan  
 Transportation Plan

Source: City of Owen Sound Official Plan, Schedule C  
 Transportation Plan (2021)

Figure

**3**





## Transportation Network

Source: City of Owen Sound Official Plan, Schedule D  
Active Transportation Plan and Trails Master Plan (2021),  
Owen Sound Transit Map

### 3. Proposed Development

The Proposed Development consists of two phases of development (see **Figure 5 and 6**). Phase 1 consists of two, four storey apartment buildings and 10 townhouse blocks. 78 units are proposed for each apartment building, and 87 townhouse units are proposed. The second phase of the Proposed Development is three, four storey apartment buildings with 78 units in each. A total of 477 units are proposed, resulting in an overall site density of approximately 95 units per net hectare. Through the Proposed Development, 10<sup>th</sup> Avenue East will be extended as a 20.5 metre collector road.

#### Phase 1

The two apartment buildings proposed (Buildings A and B) are four storeys in height, each providing 78 units. Building A is located along 10<sup>th</sup> Street East, framing the roadway, while Building B is located along the southern property line. A surface parking lot containing 195 parking spaces is located between the two buildings. Primary entrances to the apartment buildings are located towards the parking lot, providing a convenient access point for residents. Secondary entrances are located at each end of the apartment buildings.

A total of 87 townhouse units are proposed, provided in 10 blocks:

Block	Units
1-3	9
4-6	10
7	6
8-10	8

Blocks 1 through 6 are efficiently laid out in a back-to-back format, while blocks 7 to 10 are located along the southern lot line with their rear yards to the lot line. Each townhouse unit proposed features a minimum 7 metre rear yard and 6 metre setback from the interior road with a driveway for parking. A landscaped setback is provided to the southern lot line, along the rear yard of townhouse blocks 7-10.

A common outdoor amenity area is proposed at the southwestern corner of the Subject Site. This 872 m<sup>2</sup> space provides a transition to the existing hospice centre located adjacent this area of the Subject Site. Pedestrian walkways are proposed throughout the Proposed Development, providing safe and convenient circulation.

#### Phase 2 and Collector Road

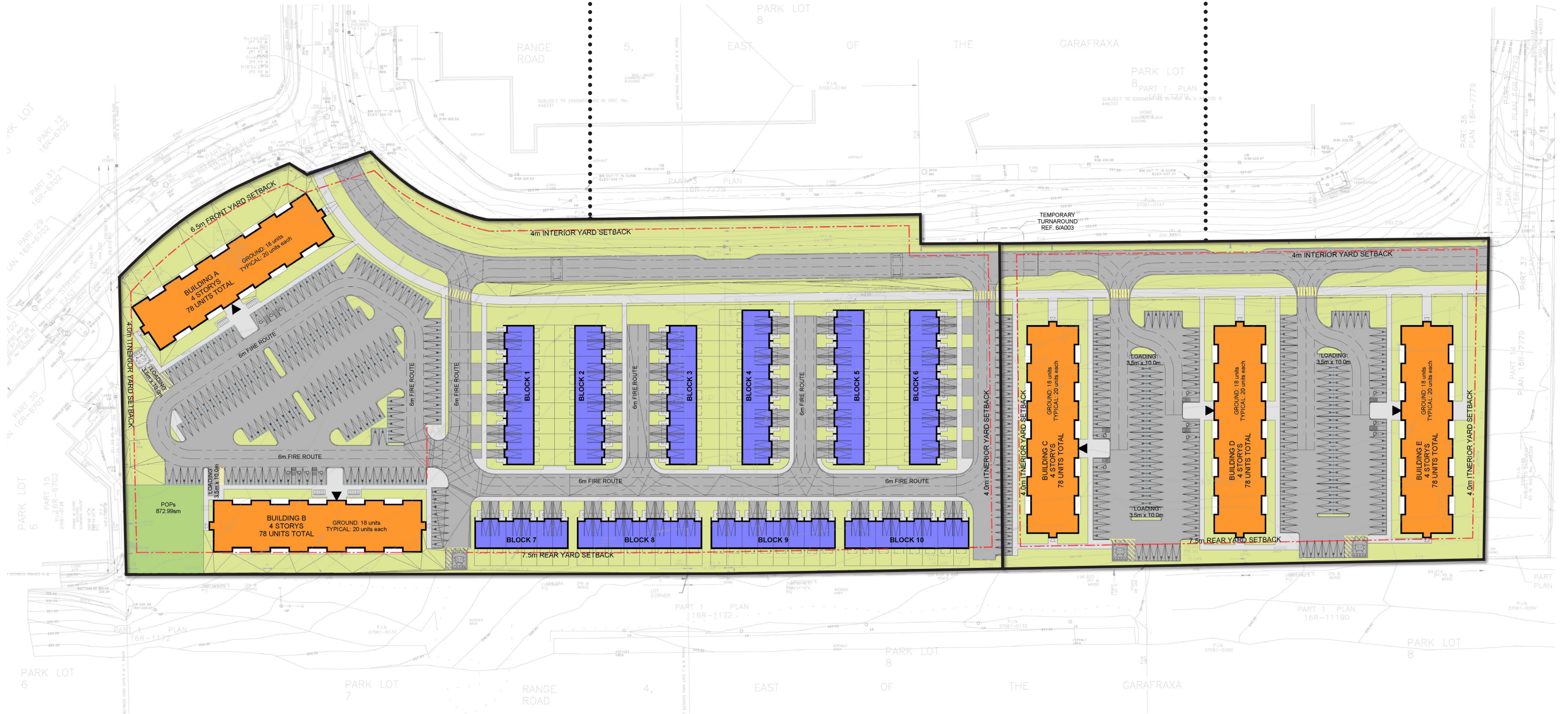
The second phase of the Proposed Development is illustrative in nature. This phase consists of three, four storey apartment building with 78 dwelling units each, for a total of

234 units. Parking is provided in central surface parking lots, conveniently sized to minimize distances from the apartments and their parking. Pedestrian connections are presented on each side of the apartment buildings, connecting the entrances of the buildings to the public realm.

The collector road is a built at a width of 20.5 metres, with narrowed section to calm traffic. A sidewalk is proposed along the developed portion of the Subject Site, leaving the northern side of the right-of-way. Two access/egress points to the developed area of the Subject Site are proposed from this roadway.

# PHASE 1

# PHASE 2



Overall Site Plan  
Source: January (2023)

Figure  
**5**



Phase 1 - Site Plan  
Source: January (2023)

Figure  
**6**



## 4. Policy and Regulatory Context

### 4.1 Provincial Policy Statement (2020)

The Provincial Policy Statement (“PPS”) provides land use planning policy on matters of provincial significance. The 2020 PPS came into effect on May 1, 2020. The overriding vision of the PPS states that *“the long-term prosperity and social well-being of Ontarians depends on maintaining strong, sustainable and resilient communities for people of all ages, a clean and healthy environmental, and a strong and competitive economy”*.

#### **Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns**

Under Section 1 of the PPS, Building Strong Healthy Communities, Section 1.1 provides policy direction on managing and directing land use to achieve efficient and resilient development and land use patterns:

- Policy 1.1.1 states *“Healthy, liveable and safe communities are sustained by:*
  - a) *promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
  - b) *accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
  - c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
  - d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
  - e) *promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
  - g) *ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;”*
- Policy 1.1.2 directs planning authorities to ensure that sufficient land is made available to accommodate an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 25 years, informed by provincial

guidelines. Within Settlement Areas, planning authorities are directed to make sufficient land available through intensification and redevelopment and, if necessary, designated growth areas.

- The preamble of Section 1.1.3 states that *“the vitality and regeneration of settlement areas is critical to the long-term economic prosperity of our communities”* and that it is in the community interest *“to use land and resources wisely, to promote efficient development patterns, protect resources, promote green spaces, ensure effective use of infrastructure and public service facilities and minimize unnecessary public expenditures”*.
- Policy 1.1.3.1 directs planning authorities to make Settlement Areas the focus of growth and development.
- Policy 1.1.3.2 directs planning authorities to base land use patterns on a range of opportunities for intensification and redevelopment and of densities and a mix of uses which:
  - a) *“efficiently use land and resources;*
  - b) *are appropriate for and efficiently use the infrastructure and public service facilities which are planned or available;*
  - c) *minimize negative impacts on air quality and climate change and promote energy efficiency;*
  - d) *prepare for the impacts of climate change;*
  - e) *support active transportation;*
  - f) *are transit-supportive, where transit is planned, exists or may be developed;*
- Policy 1.1.3.3 states that *“Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.”*
- Policy 1.1.3.4 encourages planning authorities to promote appropriate development standards which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.
- Policy 1.1.3.5 requires planning authorities to establish and implement minimum targets for intensification and redevelopment within built-up areas, based on local conditions.
- Policy 1.4.3 directs planning authorities to *“provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:*

- a) *establishing and implementing minimum targets for the provision of housing which is affordable to low and moderate income households;*
  - b) *permitting and facilitating:*
    - 1. *all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities; and*
    - 2. *all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3*
  - c) *directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;*
  - d) *promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed; and*
  - e) *requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations;*
  - f) *establishing development standards for residential intensification, redevelopment and new residential development which minimize the cost of housing and facilitate compact form, while maintaining appropriate levels of public health and safety.”*
- Policy 1.6.7.2 promotes making efficient use of existing and planned infrastructure, including transportation demand management strategies where feasible.
  - Policy 1.6.7.4 promotes land use patterns, densities and mixing of uses to minimize the length and number of vehicle trips and support the use of transit and active transportation.
  - Policy 1.7.1 encourages planning authorities to support long-term economic prosperity by:
    - a) *“promoting opportunities for economic development and community investment-readiness;*
    - b) *encouraging residential uses to respond to dynamic market-based needs and provide necessary housing supply and range of housing options for a diverse workforce;*
  - Policy 1.8.1 requires planning authorities to “*support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions and climate preparing for the impacts of a changing climate through land use and development patterns which:*
    - a) *promote compact form and a structure of nodes and corridors;*

## 4.2 County of Grey Official Plan

The County of Grey Official Plan (“County OP”) was approved by the Province on June 6, 2019. It expresses the following vision for the county: *“to be the place where people feel genuinely at home and naturally inspired – enjoying an exceptional blend of active healthy living and economic opportunity”*. To implement this vision, the County OP is structured around five key themes. Of particular relevance to the Subject Applications is “Develop Grey”, which promotes development focused within the designated Settlement Areas. “Live Grey” is also relevant as it promotes healthy communities, community inclusion, housing needs from an affordability standpoint.

### Managing Growth

Grey County has adopted an amendment to the County OP to update the residential and employment growth forecasts, and other housekeeping matters. Mapping and text changes are implemented through Official Plan Amendment #11 to the County OP.

This proposed amendment provides population, household, and employment projection to 2046:

#### Population

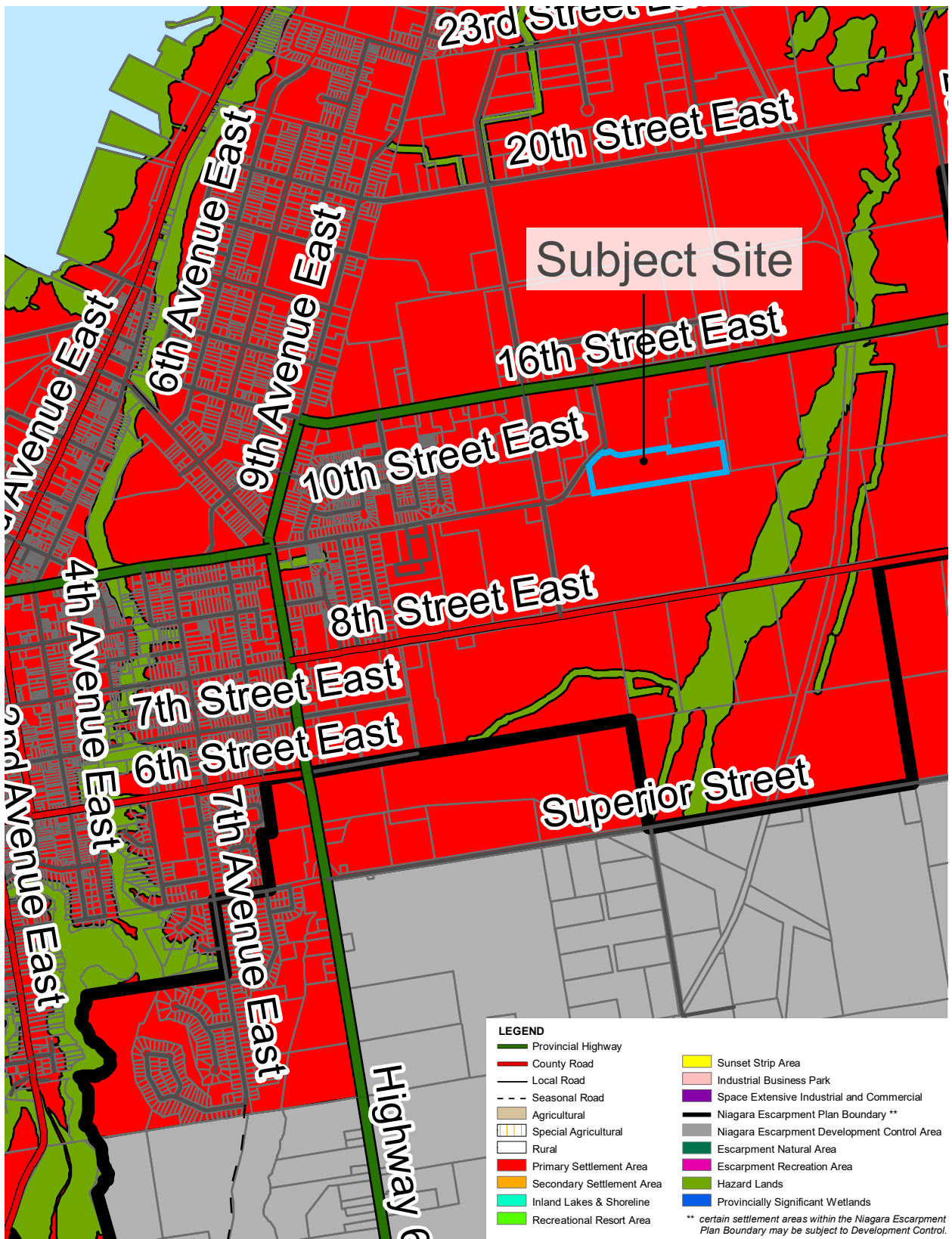
	2021	2026	2031	2036	2041	2046	Growth
Owen Sound	22,510	23,100	23,590	24,080	24,540	24,910	2,400
Grey County	103,320	108,650	113,450	118,150	122,680	127,130	23,810

#### Households

	2021	2026	2031	2036	2041	2046	Growth
Owen Sound	10,140	10,480	10,750	11,000	11,220	11,270	1,130
Grey County	43,530	46,550	49,160	51,550	53,650	55,570	10,040

### Settlement Area Land Use Types

Section 3.3 of the County OP establishes the land use types of the County. According to Schedule A, the City of Owen Sound is classified as a Primary Settlement Area (see **Figure 7**).



County Land Use  
 Source: The County of Grey Official Plan, Secondary Schedule - Land Use Types, Map 1h (2019)

This section promotes efficient development patterns and growth in primary settlement areas, stating:

- *Policies of this Plan will promote development forms and patterns which minimize land consumption and servicing costs. This will help ensure development is compact in form and promotes the efficient use of land and provision of water, sewer and transportation, and other services.*
- *Settlement areas include cities, towns, villages, and hamlets, as well as, growth areas along our shorelines and in our recreational areas. Settlement areas within the County vary in terms of size, density, population, economic activity, diversity, and intensity of land uses, service levels, and types of infrastructure available.*
- *Primary Settlement Areas – larger settlements with full municipal servicing, and a wide range of uses, services, and amenities which are intended to be the primary target for residential and non-residential growth.*

### **General Policies Affecting Settlement Area Land Use Types**

Section 3.4 provides further policy direction for Settlement Areas. Policy 3.4.1 establishes an overall intensification target for new growth at 15%. This target is further confirmed for Owen Sound on Table 6 of the County OP.

- Policy 3.4.6 directs that development within growth areas should occur adjacent to the existing built-up area and will have a compact form, mix of uses, and densities that allow for the efficient use of land, infrastructure, and public service facilities.
- Policy 3.4.14 encourages that development of communities occur with a wide range of housing types, including detached, semi-detached, townhouse, and apartment units, be provided, along with a mix of affordable housing, including second units and special needs housing, range of alternative locations, forms and densities of housing, and price ranges to meet a variety of housing needs.
- Policy 3.4.15 establishes that development within the built-up areas may be of higher density to achieve the policy directives of this Plan but should be compatible with adjacent residential areas. The local municipalities may explore means to ensure compatibility through measures such as transitional densities, built form, and land uses.
- Policy 3.4.16 directs that settlement form and building design must consider conservation in energy, water and wastewater management, the current use or eventual introduction of public transit, the integration of paths and trails, bicycle routes, a compact and convenient design which encourages walking, the incorporation of natural heritage features and areas, public safety including the impact on crime prevention, and the preservation of public access to shorelines.

## Primary Settlement Areas

Section 3.5 provides further policy direction specific to the Primary Settlement Areas of the County. The preamble of this section states:

*“Primary Settlement Areas are areas suitable for high intensification targets, public transit services, and have full municipal services. Municipalities with primary settlement areas will, in their official plans, identify and plan for intensification within these areas. The development of high quality urban form and open public spaces within these areas is also encouraged through site design, and urban design standards, to create attractive and vibrant places that support walking and cycling for everyday activities and are transit-supportive.”*

- Policy 3.5.2 directs that Primary Settlement Areas will have land use policies and development standards in accordance with local official plans.
- Policy 3.5.3 promotes the development of Primary Settlement Area land use types for a full range of residential, commercial, industrial, recreational, and institutional land uses. These areas will be the focus of the majority of growth within the County.
- Policy 3.5.5 establishes a minimum development density of 25 units per net hectare for new development in the City of Owen Sound.
- Policy 3.5.6 directs that intensification opportunities are strongly encouraged within Primary Settlement Areas. Municipalities must develop and adopt intensification strategies to ensure that the residential intensification targets identified in Section 3.4.1 of the Official Plan are met. Intensification strategies in Primary Settlement Areas shall enable:
  - a) Brownfield redevelopment;
  - b) ‘As-of-right’ permissions in official plans and zoning by-laws for second units;
  - c) The development of vacant and/or underutilized lots within previously developed areas;
  - d) The expansion or conversion of existing buildings.

New construction through intensification should occur in a manner that takes into account the existing built and physical environment and is compatible with the surrounding land uses.

## Housing Policy

Section 4.1 of the County OP establishes the County’s housing policies. The preamble of this section provides direction on both the supply and variety of housing anticipated for the County. The housing variety section states:

*“The County will aim to provide a variety of housing types to satisfy the present and future social, health, safety, and well-being requirements of residents. In doing so, we want to prioritize housing accessibility and affordability.*

*This plan encourages housing opportunities that address the needs of seniors and persons with physical, sensory, and mental health disabilities. We want people to be able to remain in a neighbourhood as housing needs change over time. It will be important to consider experimental housing types (i.e. life lease, cooperative housing, or “life-style” communities). These neighborhoods and facilities should be oriented as being age-friendly and encouraged within urban settings with appropriate services.*

*New residential developments will be promoted at densities which efficiently use available servicing (subject to Section 8.9) and are appropriate to site conditions and existing patterns of development.”*

Further to the housing variety direction, residential intensification policies are also provided. The policies pertinent to the Subject Applications include:

- Policy 4.1.1 directs for the support of increased densities in newly developing areas with a broad mix of housing types and integrated mixed-use developments, accessible housing and integrated services, and housing forms.
- Policy 4.1.6 encourages intensification which results in new rental accommodation.
- Policy 4.1.9 directs for development to be cost effective, environmentally sound, sustainable, and compatible with existing uses.
- Policy 4.1.10 establishes that adequate infrastructure is, or will be, established to serve the anticipated development.

### **4.3 City of Owen Sound Official Plan**

The City of Owen Sound Official Plan (the “City OP”) reflects the policy direction of the County OP and contains further policy direction for land use and development at the localized scale. The City OP contains goals, objectives and policies to manage and direct physical development within the context of social, economic, built and natural environmental matters in the City. The City OP was first adopted in 1984 and was subsequently amended in 2006 and 2012. In 2019 the City initiated a review of the Official Plan that included analysis of existing policy, trends and issues; consideration of recommendations from City-approved Master Plans; and, public consultation. The process led to the preparation of an amended Official Plan in 2021, which was approved with modifications in January 2022.

#### **Vision, Goals, and Objectives**



Section 2 of the City OP establishes the vision, goals, and objectives of the City. The vision of the City OP states that *“The City will plan to evolve as a centre for growth, opportunity and innovation while ensuring that equity, inclusion, diversity, sustainability, resiliency, and quality of life are considered in every decision.”*

One of the goals of the City OP is managing growth. This goal is to *“promote and encourage the growth and development of the City through a planning framework that supports sustainability, healthy communities, planned growth and quality of life for all residents of the City.”*

This goal is supported by several objectives, including:

- a) *To plan for a complete community and promote a healthy, active City for the benefit of all residents and visitors.*
- b) *To accommodate the population and development anticipated for the City within the planning period in a sustainable, compact urban form and to avoid land use patterns that may cause environmental or public health and safety concerns or promote non-compatible land uses.*
- c) *To maintain and promote the importance of the River District Commercial and Harbour as a focus of growth and investment and a key cultural and tourism resource in the City.*
- d) *To increase housing supply by identifying and promoting opportunities for intensification and redevelopment where appropriate, and plan for a full range and mix of housing options in terms of dwelling types, densities and tenure.*
- e) *To consistently apply a diversity lens to growth management through planning for persons having diverse abilities and circumstances.*

## **Land Use**

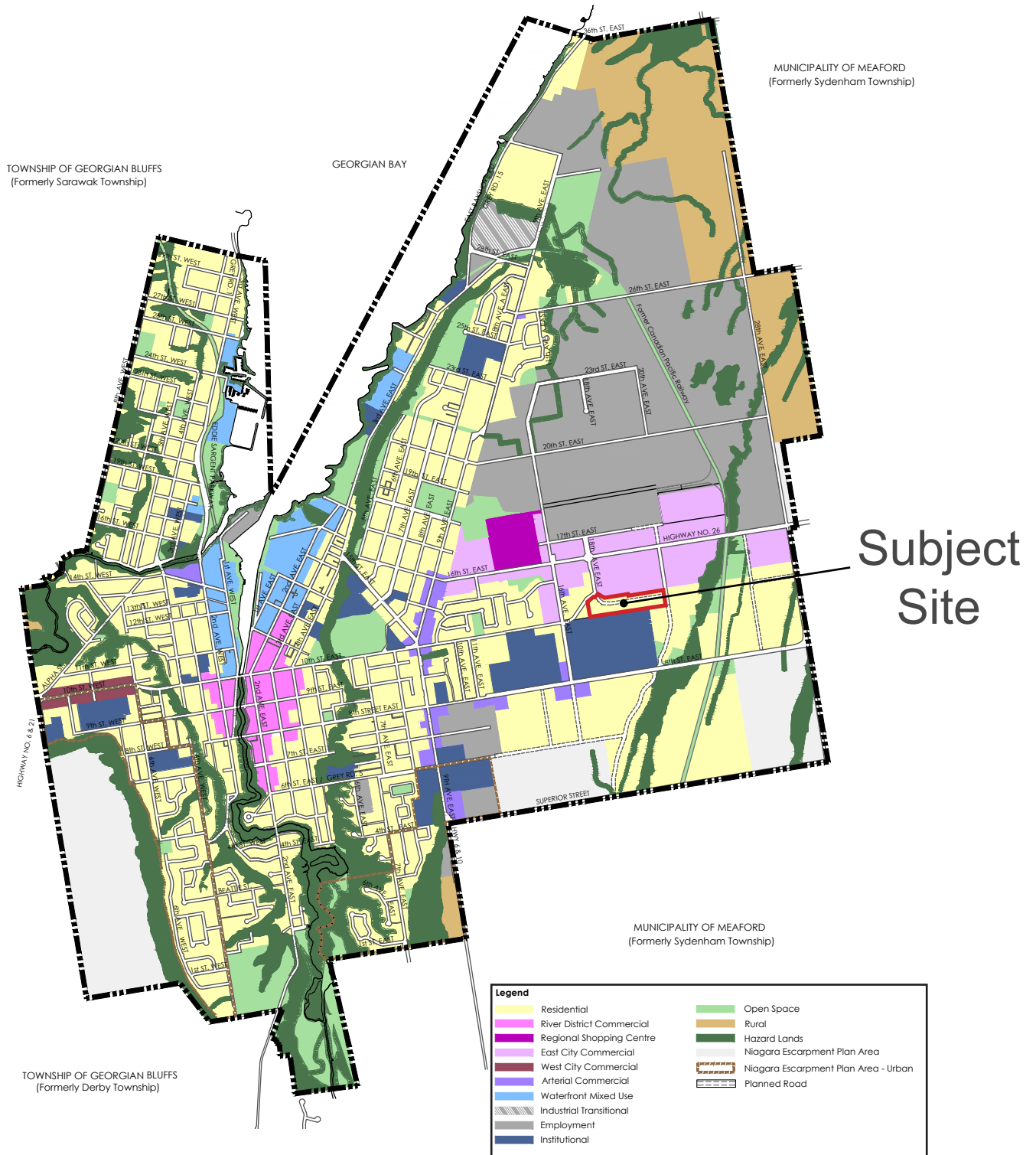
Section 3 contains the policies for the land use designations of the City OP. As noted on Schedule A of the City OP, the Subject Site is designated “Residential” (see **Figure 8**).

The preamble of this section states that *“Residential uses may include single detached, semi-detached, various forms of townhouse, and apartment buildings. Residential uses shall be permitted at low, medium and high density”*. The density of the Proposed Development is approximately 95 units per net hectare, which is noted as being within the high-density residential bracket according to policy 3.1.2.1.

Further to this density policy, policy 3.1.2.5 states that *“density shall be allocated in residential areas in accordance with the following:*

- a. *Lands shall have access to hard services at sufficient capacity for the intended density.*

- b. *Adequate soft services such as parks, schools, emergency services, transit and similar shall be available as required to support the intended density.*
  - c. *Lands intended for medium and high-density uses should have direct access to collector or arterial roads and proximity to compatible land uses including but not limited to local institutional and neighbourhood commercial uses.”*
- Policy 3.1.3.1 establishes that residentially designated land shall have access to full municipal services, including piped water supply, storm drainage and sanitary sewer systems, installed according to requirements of the City and other applicable agencies.
  - Policy 3.1.4.1 establishes that an objective of the City OP is to provide for a full range of housing types and densities required to meet projected requirements of current and future residents of the regional market area. Regional market area refers to an area, generally broader than a lower-tier municipality that has a high degree of social and economic interaction.
  - Policy 3.1.5.1 directs that the City will support a range of attainable housing development in proximity to transit and amenities to increase mobility and accessibility to goods and services, healthy food retailers, commercial areas, employment, medical and health facilities, recreation, transit, and trails.
  - Policy 3.1.8.1 establishes that the City will support innovative residential development that meets the City’s housing objectives.
  - Policy 3.1.8.2 directs that housing intensification is supported subject to the following policies:
    - a. *“The proposed development meets locational and other criteria of this Plan.*
    - b. *The type, size and scale of the proposed development is compatible with adjacent development and planned land use.*
    - c. *The existing infrastructure, including sewer and water services, can support additional development.*
    - d. *The existing community and recreational facilities, such as schools and parks are adequate to meet the additional demand.*
    - e. *Required parking can be accommodated.*
    - f. *The local road network can accommodate any additional traffic.”*



**City of Owen Sound Official Plan Land Use**  
 Source: City of Owen Sound Official Plan, Schedule A Land use (2021)

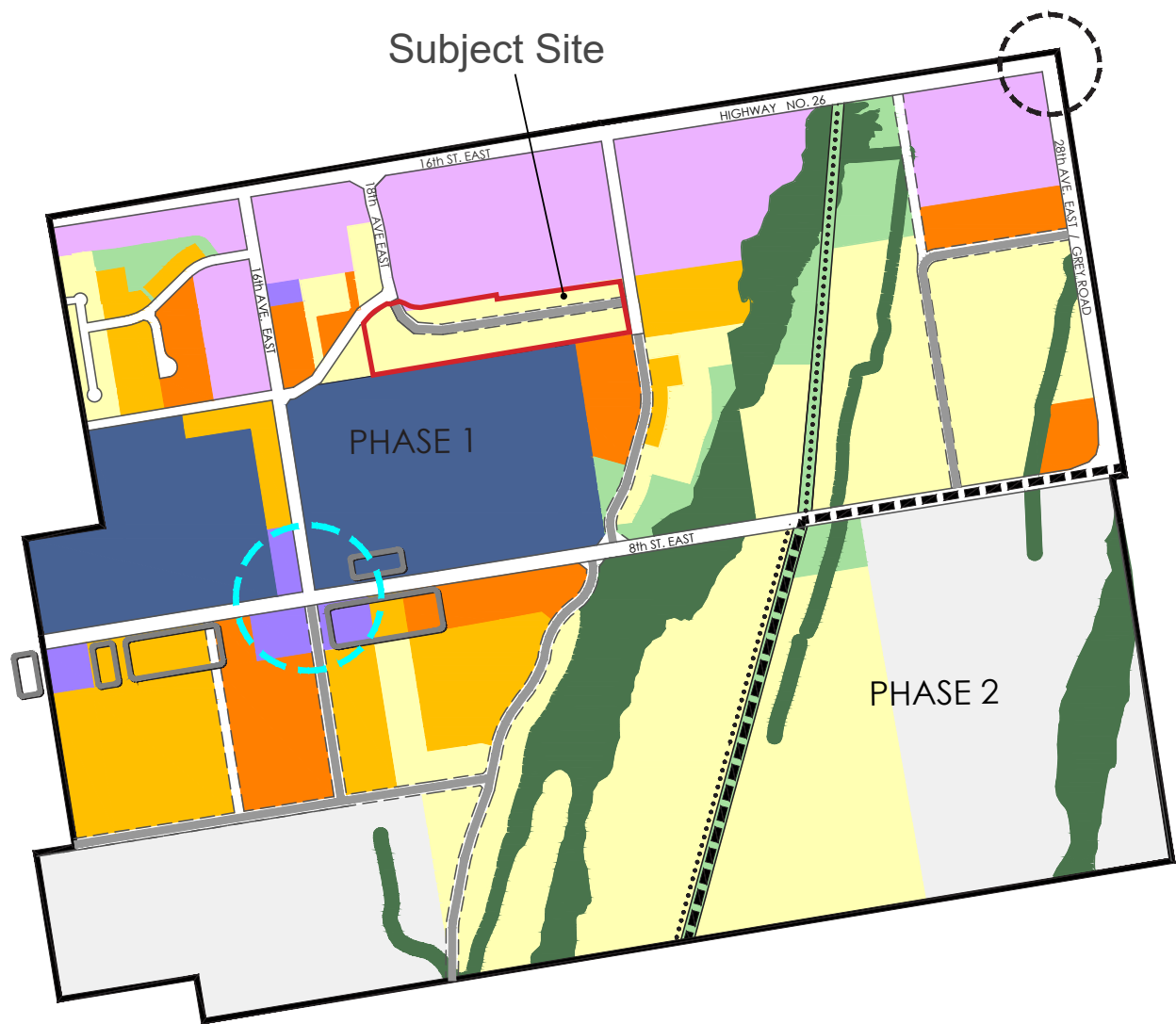
Figure  
**8**

## Planning Areas

Section 4 of the City OP contains secondary plans for certain areas of the City identified as Planning Areas, as shown on Schedule B (see **Figure 9 and 10**). The Subject Site is identified as being within the Sydenham Heights Planning Area (Phase 1 area). The policies of the Sydenham Heights Planning Area are provided under Section 4.2 of the OP.

- Policy 4.2.2.4 directs that the permitted uses in the Sydenham Heights Planning Area include a range of low density, medium density and high density dwelling types.
- Policy 4.2.2.5 further directs that the overall density target for the Planning Area is not less than 25 units per net hectare. This target shall be planned for in an equitable manner across the entire secondary plan area.
- To minimize potential land use conflicts between lands designated medium density and medium/high density, Policy 4.2.3.3 establishes that residential development should be adequately buffered and/or physically separated. This can be accomplished with screening, fencing, tree buffering and landscaping. The City may require a study to assess any noise impacts.
- Regarding the road system of the Planning Area, Policy 4.2.5.1 establishes that all lands shall be developed in general conformity with the schematic road system plan illustrated on Schedule 'A2', subject to the policies contained herein. For local and collector roads, the road system plan may be considered illustrative only and changes to the location and configuration of such streets shall be permitted without amendment to this Plan, provided the planned street does not adversely impact the development ability of adjoining lands or the general traffic flow system provided, and the layout is in accordance with suitable design principles.
  - Policy 5.1.3.14 also provides direction for Collector Roads: Collector roads are to provide the two functions of carrying moderate levels of traffic between points of origin and the arterial road system and to provide access to abutting properties.
    - a) Collector roads are to be designed in accordance with the general design standards in Appendix 'A'.
    - b) Collector roads may be constructed with two to four lanes in width.
    - c) The minimum road allowance width for City collector roads shall generally be 25 metres.
    - d) Collector roads are to ensure that free flow of traffic is provided but with appropriate traffic control measures to ensure such traffic does not impact abutting lands uses to the extent that development opportunity is significantly reduced.
    - e) Direct access to collector roads shall be permitted subject to geometric design considerations.

- f) On-street parking may be permitted on collector roads where location and time restrictions are enforced in specific instances, for example, adjacent to schools or during peak periods of traffic demand.
- g) Sidewalks shall generally be constructed on both sides of collector roads.
- Further, as per Policy 4.2.5.4, direct access from individual lots in low and medium density residential developments onto connecting links and arterial streets is strongly discouraged.
- Regarding services, Policy 4.2.6.1 directs that all development within the Sydenham Heights Planning Area shall proceed on the basis of full municipal wastewater and water services.
- To provide for the orderly development and extension of physical services into the Planning Area, priority development areas and phasing has been established, as per Policy 4.2.7.1. The Sydenham Heights Planning Area includes two (2) phases, as outlined on Schedules 'B' and 'A2'. The Phase 1 development is projected to accommodate a range and mix of unit types in support of the overall planned density target, excluding any infill, intensification or redevelopment. The City will monitor the unit numbers and density target through the development approvals process, as per Policy 4.2.7.2.



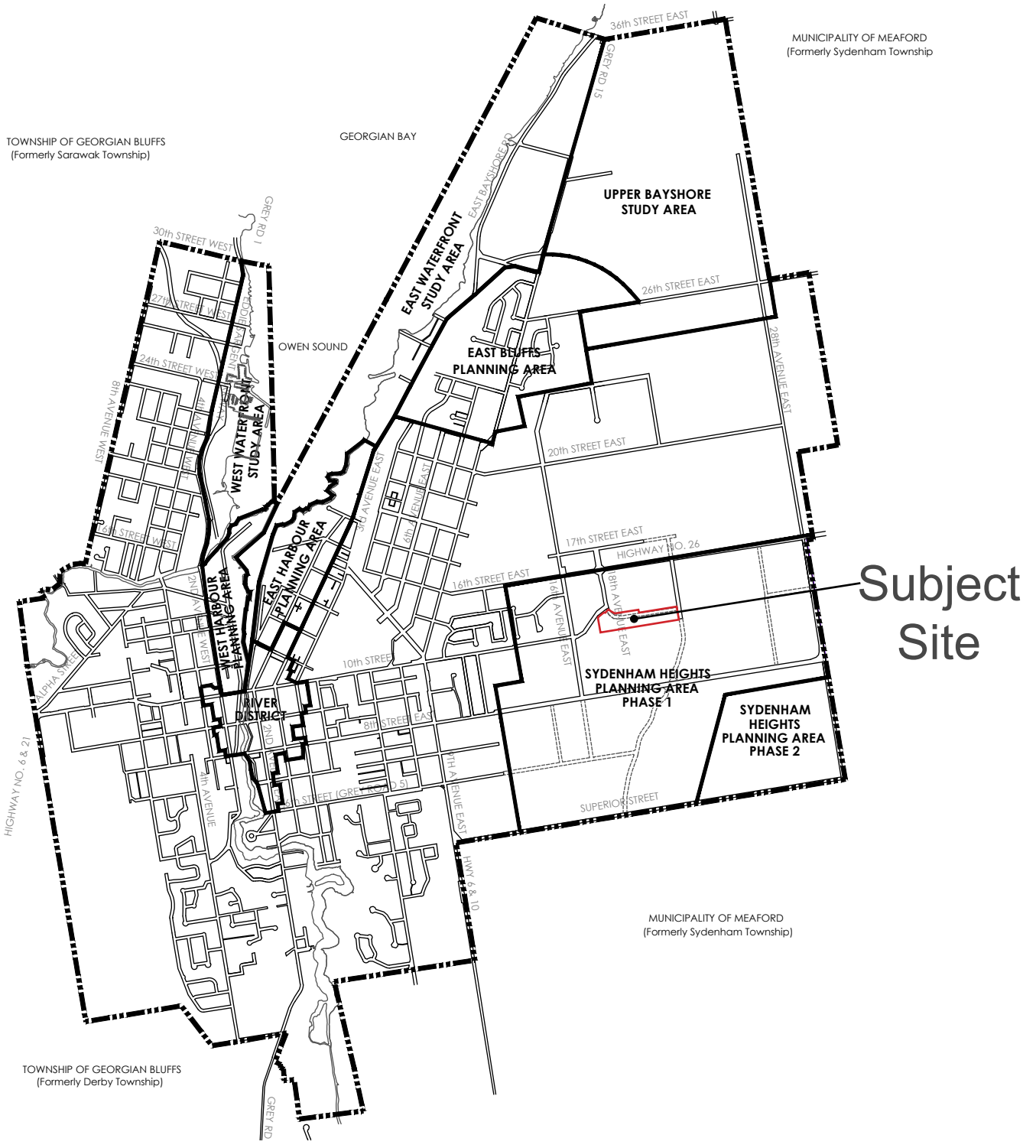
**Legend**

- Planning Area Boundary
- Low Density Residential
- Medium Density Residential
- High Density Residential
- East City Commercial
- Arterial Commercial
- Institutional
- Open Space
- Hazard Land
- Niagara Escarpment Plan Area
- Potential Location of Easthill Elevated Water Storage
- Future Collector Roads
- Future Roads
- Trail
- Phase Boundary
- Future Mixed Use Node
- Gateway



**City of Owen Sound Official Plan**  
**Sydenham Heights Phase 1 and Phase 2**  
 Source: City of Owen Sound Official Plan, Schedule A2 Land use (2017)

Figure  
9



Subject Site



Planning Areas and Study Areas  
 Source: City of Owen Sound Official Plan, Schedule B Planning Areas (2012)

Figure 10

#### 4.4 City of Owen Sound Zoning By-law

The Subject Site is currently zoned R3 (H) 14.55 (see **Figure 11**). The R3 zone is a low-density residential zoning and does not permit apartment dwellings. The ZBA application proposes to change the zoning of the Subject Site to the General Residential (R5) zone with site-specific provisions.

Zone Provision	R5 Zone		Proposed Zoning
	Townhouse	Apartment	
Minimum Lot Frontage	25 m	13.5 m	25 m
Minimum Lot Area	200 m <sup>2</sup> /unit and 900 m <sup>2</sup> total development parcel	450 m <sup>2</sup>	Townhouses: 900 m <sup>2</sup> total development parcel Apartment: 450 m <sup>2</sup>
Maximum Lot Coverage	40%	40%	40%
Minimum Front Yard Setback	6.5 m	6.5 m	6.5 m
Minimum Rear Yard Setback	7 m	7.5 m	Townhouses: 7 m Apartment: 7.5 m
Minimum Interior Side Yard Setback	1.2 m on one side, 3.0 on the other (0 m at common party wall)	1.2 m on one side, 4.0 m on the other	1.2 m on one side, 4.0 m on the other
Minimum Exterior Side Yard Setback	3 m	3 m	1.5 m
Maximum Building Height	10 m	12 m	15 m
Maximum Density	0.6 FSI	1.0 FSI	1.0 FSI

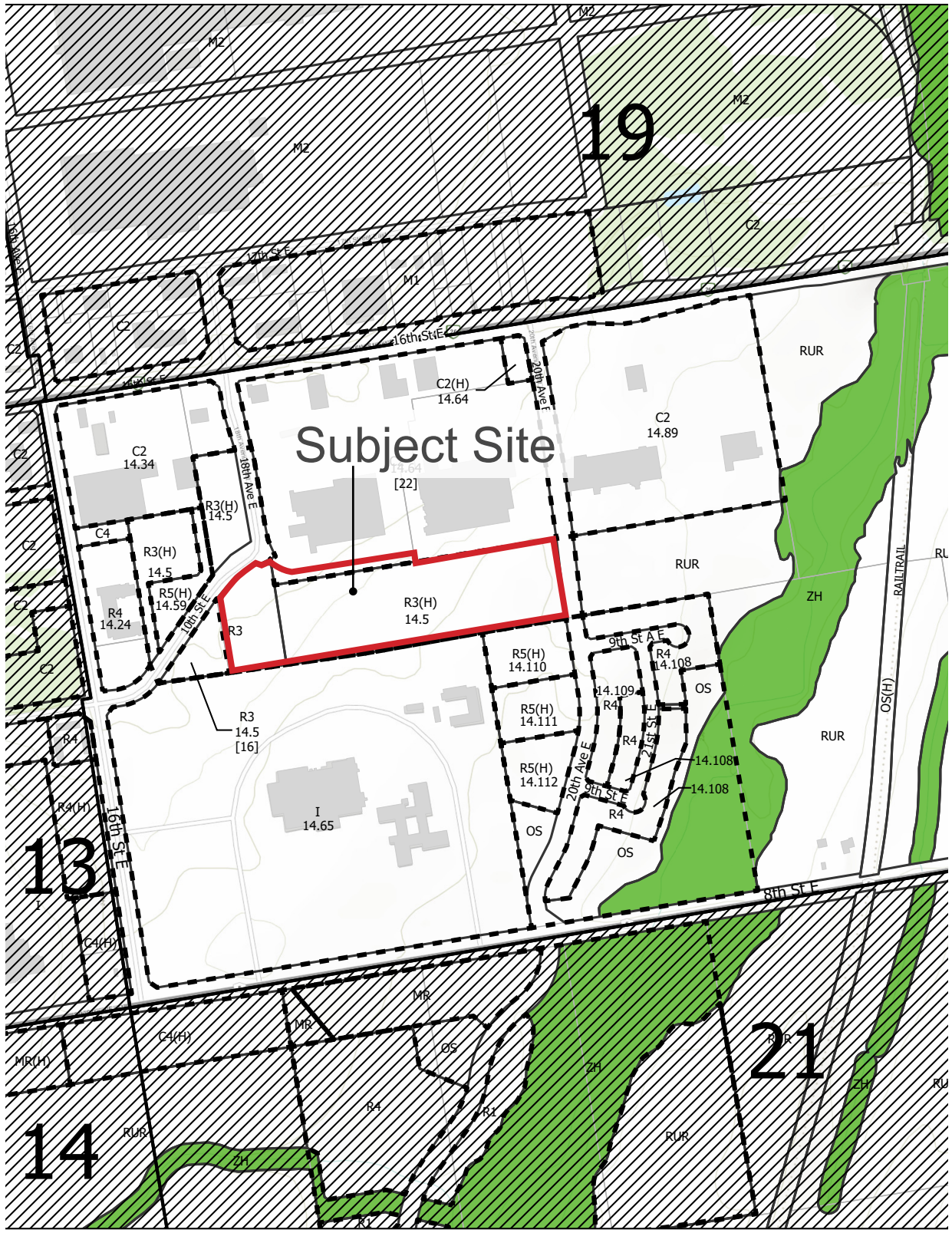
Parking requirements are provided in Section 5.18 of the Zoning By-law. For apartment dwellings, parking is required at a rate of 1.25 spaces per unit. Cluster townhouses are required to provide 1.25 parking spaces per dwelling unit. Section 5.20 provides the required bicycling parking spaces, which is 10% of the required vehicle parking spaces.

The Proposed Development provides 156 apartment dwelling units and 87 townhouse dwellings. Therefore, a total of 195 vehicle spaces are required for the apartment use, and 109 spaces are required for the townhouse uses. The 195 required spaces for the apartment dwellings are located in a surface parking lots, while the cluster townhouses include both a garage and driveway, resulting in two parking spaces per unit and total of 174 parking spaces. The Phase 2 portion of the Subject Site provides 234 apartment dwelling units,



resulting in a requirement of 293 parking spaces. An amendment is being sought for this provision, as detailed further in this Report.

20 bicycle parking spaces are required for the apartment, and 11 for the townhouses. These parking requirements are met by the apartment buildings. The townhouse units can accommodate bicycle parking in each unit's garage, with the Subject ZBA implanting this approach.



City of Owen Sound Zoning By-Law Schedule A

## 5. Technical Report Summaries

### 5.1 Transportation Impact Study and Transportation Plan

A Transportation Impact Study (“TIS”), including Transportation Plan, was prepared by CGH Transportation, which includes an analysis of existing traffic conditions, a description of the Proposed Development traffic, traffic forecasts for 2027 and 2032 (taking into consideration surrounding developments), and recommendations to mitigate future traffic conditions.

The TIS finds that the Proposed Development is projected to generate new two-way vehicle volumes of 187 and 198 during the weekday AM and PM peak hours, respectively. Using the 2022 existing traffic volumes, an operational analysis of existing conditions was undertaken. Through this analysis it was determined that all Study Area intersections operate with good overall line of sight and delay. The queues are also contained by the storage lengths with the exception of the shared westbound through/right lane at the intersection of 16th Street East and 18th Avenue East in the AM peak period, and the northbound left turn lane at the same intersection in PM peak period.

The Ministry of Transportation Ontario (MTO) Geometric Design Standards for Ontario Highways (GDSOH) has been reviewed to determine the need for an eastbound left-turn lane and westbound left-turn lane at the intersection of 8th Street East and 20th Avenue East for the 2027 future background analysis horizon. Using the GDSOH methodology and a 60 kilometre per hour design speed, it was found that an eastbound left-turn lane will be warranted. Although a dedicated westbound left-turn lane was not warranted based on the GDSOH methodology, it has been assumed that one would be included at the intersection of 8<sup>th</sup> Street East and 20<sup>th</sup> Avenue East due to the wider geometry of the intersection.

The 2027 future background traffic volumes, including the background growth, find that turning movements operated with a reasonable line of sight and delay. Through this analysis it was determined that all Study Area intersections operated with good overall line of sight and delay. The queues were also contained by the storage lengths, with the exception of the shared westbound through/right lane at the intersection of 16th Street East and 18th Avenue East in both the AM and PM peak periods.

The 2032 future background traffic volumes, including the background growth, find that turning movements operated with a reasonable line of sight and delay. Through this analysis it was determined that all Study Area intersections operated with good overall line of sight and delay. The queues were also contained by the storage lengths, with a couple exceptions.

With the addition of the 2027 future total (Phase 1) site traffic volumes to the Study Area intersections, the intersections operate in a similar manner to the 2027 future background analysis horizon. With the addition of the 2032 future total (Phase 1 and Phase 2) site traffic volumes to the Study Area intersections, the intersections operate in a similar manner to the 2032 future background analysis horizon with the exception of the intersection of 10th Street East and 18th Avenue East. At this intersection a LOS F, V/C ratio greater than 1.00, high delays and extended queues were noted in the PM peak period. Mitigation measures are recommended in the form of a separated eastbound left-turn lane and all-way stop-control at the intersection. This recommendation is also intended for the 2027 future total analysis horizon.

## **5.2 Functional Servicing and Stormwater Management Report**

A Functional Servicing and Stormwater Management Report was prepared by Stantec to provide a servicing opinion regarding the availability of existing municipal infrastructure to support the Proposed Development.

The report finds that:

- Stormwater quantity control consisting of underground storage chambers with orifice plates will be provided to meet the design intent of the overall stormwater management strategy.
- The onsite stormwater detention provided will protect the function of existing downstream stormwater management infrastructure.
- Stormwater quality treatment is to be provided via oil-grit separator units.
- The proposed stormwater management strategy will allow for the Phase 1 and Phase 2 developments to occur at different times.
- A 250 mm diameter watermain exists in the corner of 10th Street East and 18th Avenue East right-of-way adjacent to the site. Water supply for the property can be provided by connection to this municipal watermain.
- Ten (10) private fire hydrants will be installed within the site as required to provide firefighting coverage for the proposed buildings.
- The Proposed Development can be serviced for sanitary drainage by connecting to the existing 250mm diameter sanitary sewer along 18th Avenue East. A sanitary analysis of the downstream municipal sewer system was completed. The receiving sewer immediately downstream of the development has adequate capacity to accept the increased flows from the development. Although the analysis shows that some isolated existing downstream sewers may be over capacity, it appears that the municipal sewer network can accommodate the proposed development.

### **5.3 Archaeological Assessment**

A Stage 1 and 2 Archaeological Assessment was undertaken by Archeoworks Inc for Phase 1 of the Proposed Development. The Stage 1 Assessment provides information about the property's geography, history, previous archaeological fieldwork and current land condition, and evaluates in details the property's archaeological potential, which supports recommendations for a Stage 2 survey for all or parts of the property. The Stage 2 Assessment documents all archaeological resources on the property, determines whether the property contains archaeological resources requiring further assessment, and recommends appropriate Stage 3 assessment strategies for archaeological sites identified.

Stage 1 background research established elevated potential for the recovery of archaeologically significant materials within the study area due to the proximity of documented pre-ca. 1900 Euro-Canadian settlement. The study area was subsequently subjected to a Stage 2 AA as required by the 2011 Standards and Guidelines for Consultant Archaeologists ('2011 S&G') published by the Ministry of Citizenship and Multiculturalism (MCM). The Stage 2 property survey identified portions of the study area as exhibiting deep and extensive land alterations that have removed archaeological potential (e.g., previous construction grading conditions). Saturated land conditions and steeply sloping terrain were also identified. A systematic survey was not undertaken for these areas due to their low to no archaeological potential classification.

The remainder of the study area, consisting of a woodlot and areas of overgrown vegetation, was subjected to a test pit form of survey at five-metre intervals. During the test-pit investigation, a rectangular depression representing a potential unmarked burial feature was identified by a Saugeen Ojibway Nation (SON) monitor. At the request of SON, further archaeological assessment of this feature is recommended. Further assessment with SON was attempted, but could not be completed due to weather. SON agreed that the submission can proceed with Stage 3 being a condition of approval.

Based on the findings of the Stage 2 Archaeological Assessment, a Stage 3 Assessment is recommended.

### **5.4 Noise Impact Study**

A Noise Impact Study was prepared by RWDI to provide a preliminary assessment of the environmental noise conditions of the Subject Site. The Study finds that due to the transportation sound levels at the plane of the façade, central air conditioning is recommended for Building A to allow for windows and doors to remain closed as a noise mitigation measure. Prospective purchasers or tenants should be informed by a warning clause. Further, it is advised that a ventilation system to facilitate windows being closed be installed.

The potential noise levels from stationary sources of sound were evaluated. Based on the noise modeling results with the noted ventilation requirements, the proposed development is not anticipated to infringe on the compliance of any commercial or industrial operations with environmental noise permits (ECA or EASR), nor cause infractions against the local noise by-law.

At this stage in design the noise levels produced by the development on itself, and its surroundings could not be quantitatively assessed. However, the effect on both the building itself and its surroundings is expected to be feasible to meet the applicable criteria. It is recommended that the building design is evaluated prior to building permit to ensure that the acoustical design is adequately implemented in order to meet the applicable criteria.

Based on the results of the analysis including implementation of the recommendations included with this assessment, the Study finds that the Proposed Development is feasible from an environmental noise perspective.

## **5.5 Urban Design Brief**

An Urban Design Brief was prepared by GSP Group to assess the Proposed Development against the goals and objectives of City of Owen Sound. This Brief includes an analysis of Section 8 of the OP, which contains the City's design policies.

The Brief finds that the urban design attributes of the Proposed Development reflect principles of good urban design and fit sensitively with the character of the City of Owen Sound, demonstrating a design that is functional, accessible, attractive, safe, sustainable, and cost-effective. The thoughtful consideration of the organization of the site layout in terms of circulation, interfaces, linkages, mix of building typologies and landscaping will support quality public spaces. The design will incorporate high-quality architecture and landscaping and consider how the individual elements support the character of the community as a whole. The Brief concludes that the numerous design considerations proposed demonstrates conformity to the Section 8 of the OP.

## **5.6 Tree Inventory and Preservation Plan**

A Tree Inventory and Preservation Plan ("TIPP") was prepared by Kuntz Forestry Consulting Inc for the Phase 1 section of the Subject Site. The TIPP includes an inventory of the tree resources over 15cm diameter at breast height (DBH) in proximity to the Proposed Development, evaluate potential tree saving opportunities based, and document the findings.

The findings of the study indicate a total of 106 trees on and within six metres of the Subject Site. The removal of 76 trees is required to accommodate the proposed development. The remaining 30 trees can be saved provided appropriate tree protection measures are installed prior to the development.

## 6. Public Consultation Strategy

As part of the application package, the *Planning Act* requires a Public Consultation Strategy to outline opportunities for members of the public to be involved in the processing of the Subject Application. The Applicant is proposing to utilize the public process provided in the *Planning Act*.

In accordance with Sections 34(12), 34(13), 51(20) and 51(23) of the *Planning Act*, the City of Owen Sound provides public notice of the applications in the prescribed manner and holds a Statutory Public Meeting as part of a regularly scheduled Council Meeting. Any individuals or property owners that request further notification regarding the application would be formally notified by the City as to the time and location when Council will be considering the applications.



## 7. Subject Application and Planning Justification

### 7.1 Official Plan Amendment Application

The purpose of the Official Plan Amendment is to redesignate the Subject Site from the Low Density Residential designation to the High Density Residential designation. In addition to the land use designation change, a revised policy is proposed to reflect the proposed collector road configuration. Although policy 4.2.5.4 suggests an OPA is not required for an alternative collector road configuration, specific language is proposed to ensure its implementation. A Draft OPA is located in Appendix A of this Report.

### 7.2 Zoning By-law Amendment Application

The purpose of the Subject Zoning By-law Amendment application is to rezone the Subject Site to a General Residential (R5) exception zone.

The site-specific amendments sought in the Subject ZBA include:

- Recognize the Subject Site as one property for the purposes of zone conformity;
- Permit a maximum height of 15 metres;
- Permit a minimum exterior side yard of 1.5 metres for the Phase 1 portion of the Subject Site;
- Permit 0 parking spaces per cluster townhouse dwelling where a garage is present;
- Permit an FSI of 1.0 across all the Phase 1 lands; and,
- Permit a parking ratio for apartment dwellings of 1.0 spaces per unit where 1.25 spaces per unit is required for the Phase 2 portion of the Subject Site.

Housekeeping provisions are also proposed, including how future consent applications will apply, and permissions for a temporary sales centre, if required, for the development. A Draft Zoning By-law Amendment is located in Appendix B of this Report.

### 7.3 Planning Justification

#### **The Subject Applications are Consistent with the PPS:**

The Proposed Development represents the efficient development and land use of the Subject Site, aligning with policy 1.1.1. to provide a healthy, livable, and safe community. The range of residential dwelling styles proposed is consistent with the PPS direction for a range and mix of affordability and housing options. The Subject Site is near two transit routes, and as such should be developed at a density and scale supportive of transit to align with policy 1.1.1.e). The density of the Proposed Development is supportive of these transit networks.

The Subject Site is located within a settlement area, which are areas that are to be the focus of growth and development as per policy 1.1.3.1. Policy 1.1.3.2 reiterates the importance of efficiently using land and resource, appropriately and efficiently using infrastructure, prepare for the impacts of a changing climate, and supporting active transportation and transit. The Proposed Development meets this policy direction, as it will increase the density of the Subject Site to more efficiently use available land and existing municipal infrastructure. Both transit and active transportation networks are in proximity to the Subject Site, which will be supported by the scale of development. As indicated in Section 2.3 of this Report, two transit routes are located in walking distance of the Subject Site (the East Bayshore and Core routes).

It is noted that there is a lack of diversity in the housing types available in this area of the City. Statistics Canada provides City wide data on the housing type by structure. Of the roughly 9,870 dwellings in the City, more than half of these are single family dwellings. “Row Houses” represent approximately 6.7% of housing in the City, and apartments with less than five storeys are approximately 25% of the housing supply. This demonstrates the need for additional higher density housing options, and less low-density options, which the Subject Site is currently limited to. The Proposed Development will add to the mix of housing options, consistent with policy 1.1.3.3 and 1.4.3.

PPS policy 1.6.7.4 directs for the efficient use of existing infrastructure. As indicated in the FSR, the Proposed Development can be supported by existing municipal infrastructure. The Subject Site is adjacent a significant commercial node in the City, and as per PPS policy 1.6.7.4, personal vehicle trips are to be minimized.

Lastly, PPS policy 1.8.1 promotes compact built form to support energy conservation and efficiency and prepare for the impacts of a changing climate. The Proposed Development contemplates a built form more compact than the existing low-density permissions.

The Subject OPA and Proposed Development are consistent with the PPS, further aligning with provincial planning direction than the existing low-density permissions.

**The Subject Applications are in Conformity to the County OP:**

The County OP establishes that the County is expected to grow by 23,810 people over the next 30 years, with 2,400 people anticipated in the City of Owen Sound. The Proposed Development makes a significant contribution to the anticipated households required to accommodate this growth.

Subject Site is located within a Prime Settlement Area, which the County OP directs are to be the primary target for residential and non-residential use. The Subject Site is within the existing built-up area, and the proposed built forms are compact in nature. Policy 3.4.14 encourages a range of alternative locations, forms, and densities, which is implemented by the Proposed Development, offering a style of development not commonly observed in the City. Additionally, policy 3.4.15 permits higher densities within the built-up area, where development is compatible with adjacent residential uses. Residential use is located adjacent to the west of the Subject Site, which is a two-storey hospice. The layout of the Proposed Development locates an outdoor amenity area towards this use, easing the transition from the four storey apartments to the existing use.

The Proposed Development provides a density of approximately 95 units per net hectare, exceeding the minimum of 25 provided in Policy 3.5.5. Additionally, the Subject Site is currently vacant and is an underutilized property within the Primary Settlement Area. The redevelopment of the Subject Site implements Policy 3.5.6 that directs for the development of these types of properties. This is further directed through the housing policies of the County OP.

The housing policies of the County OP direct for increased densities in newly developing areas with a mix of housing types. The increase to high-density residential permissions is in conformity to this policy, and the variety of under-observed housing options proposed further aligns with policy.

The Subject OPA and Proposed Development are in conformity with the County OP, directing growth and compact development to the built-up area of the Primary Settlement Area of Owen Sound.

**The Subject Applications Meets the Intent of the City OP:**

The goals of the City OP directly promote an increase to the housing supply and to plan for a full range and mix of housing options, in terms of both tenure, density, and dwelling types. The Proposed Development will contribute 477 dwelling units in a mix of apartment and townhouse styles, directly contributing to goal of the OP.

The Subject Site is designated as Residential in the City OP. This designation permits the proposed residential development. The density of the Proposed Development is approximately 95 units per net hectare, which is noted as being within the high-density residential bracket according to policy 3.1.2.1. The Sydenham Heights Planning Area permits high-density residential uses, such as the Proposed Development, and the density proposed meets the minimum 25 per unit hectare density across the Planning Area. As noted in Policy 3.1.2.5.c) high-density uses are to have direct access to a collector road,

which is accomplished by the extension of 10<sup>th</sup> Avenue East. Further, high-density uses are to be in proximity to compatible land uses. The type, size, and scale of the Proposed Development is compatible with adjacent development and planned land use. Complimentary uses such as East Ridge and Sydenham Community Schools, mix of neighbourhood and City wide commercial and retail uses adjacent to the north, and the hospital adjacent to the south. The FSR indicates that the hard services are available at a sufficient capacity for the proposed density, satisfying both policies 3.1.2.5.a) and 3.1.3.1.

The size of the Subject Site presents an opportunity to offer a variety of built forms. Both apartment and cluster townhouses are proposed, which are an underrepresented built form in the City, particularly the townhouse dwellings. These dwellings are attainable in nature, offering a more affordable option than the typical single detached dwellings observed in the City and County, adding to the Regional market area, as directed in policy 3.1.4.1 and achieving policy 3.1.5.1. Additionally, the Subject Site is within the Phase 1 area of the Planning Area, and will bring housing to market in a timely manner.

As per policy 4.2.3.3, the Proposed Development is appropriately buffered and separated from adjacent properties. The uses to the north are generally commercial uses. Although the rear of these commercial uses is oriented towards the Subject Site, the future collector road provides a large setback to assist with transition. Grey Bruce Hospice is located adjacent the Subject Site to the west. An outdoor amenity area is provided at this corner of the Subject Site to provide an appropriate transition and buffer to this use. The Phase 2 lands abut a High Density Residential designated property, and are adjacent a Medium Density Residential designated property. This area of the Subject Site is intended to develop at higher densities, which is compatible with the Subject OPA.

Regarding transportation policies, the Subject Site is in proximity to two transit routes, which can be supported by the density proposed by the development. Additionally, the TIS confirms that the local road network can accommodate the Proposed Development. Full municipal services are available, demonstrating further conformity to City OP policies.

Regarding the configuration of the proposed Collector Road, Policy 4.2.5.1 establishes that the road system plan may be considered illustrative only and changes to the location and configuration of such streets shall be permitted without amendment to this Plan, provided the planned street does not adversely impact the development ability of adjoining lands or the general traffic flow system provided, and the layout is in accordance with suitable design principles.

Further, the proposed collector road complies with the direction for the future road system in policy 4.2.5.1, presenting an alternative design that maintains the principles of a collector

road, and no dwelling units have direct access to the roadway, as per policy 4.2.5.4. The proposed Collector Road also meets the policies presented in policy 5.1.3.14. 5.1.3.14.c) directs for Collector Roads to “generally” be 25 metres in width and policy 5.1.3.14.g) directs for sidewalks to “generally” be on both sides of the road. The proposed 20.5 width provides the desired one travel lane in each direction, and the sidewalk along the southern side of the proposed road is widened to facilitate two-way pedestrian movement in the form of a multi-use pathway.

The TIS indicates that the proposed design of the collector road is appropriate and in accordance with suitable design principles, and does not adversely impact the development ability of adjoining lands or the general traffic flow system provided in the Planning Area.

The Subject OPA and Proposed Development aligns with the vision of the City OP to evolve as a centre for growth, opportunity and innovation. High-density residential uses are appropriate at this location, supporting OP direction for these uses.

**The Site-Specific Amendments Meet the Intent of the Zoning By-law:**

The Subject ZBA proposes to rezone the Subject Site to an R5 exception zone, which is permitted under the Residential land use designation of the City OP and High Density designation of the Planning Area.

Although the Subject Site may be subject to further consent applications, it will function as a single entity, and is proposed to be treated as such for planning application purposes. Among other clarifications, additional height and a decreased exterior side yard is requested. The R5 zone permits a maximum height of 12 metres for apartment buildings. The Subject ZBA proposes to introduce a maximum building height of 15 metres to both the Phase 1 and Phase 2 lands. This is a difference of 3 metres, or close to one storey above the maximum.

Height controls are generally used to ensure that development can appropriately transition to surrounding uses and remain human in scale. The setbacks of the Proposed Development ensure that the apartment buildings do not overwhelm the surrounding properties or property itself. The height of the buildings themselves may be less than 15 metres in height; however, due to the Zoning By-law definition of height considering average grade and parapets, a slight increase is proposed to ensure the buildings are in conformity to this definition.

A reduction to the 3 metre exterior side yard is also proposed for the Phase 1 lands. The amendment is necessary due to the side door portion of the apartment building protruding from the building wall, resulting in the need for a 1.5 metre setback. Aside from this

protrusion, the main wall of the apartment building is 3 metres or more from the side lot line, consistent with the 3 metre requirement. Similarly, three end townhouse units encroach into the 3 metre setback requirement, being set back 1.8 metres. The vast majority of the exterior side yard is greater than the 3 metre requirement, maintaining the intent of the exterior side yard. The 1.8 metre setback is reflective of an urban environment, and can facilitate a safer pedestrian realm through fenestration along these building walls.

The proposed amendment to permit zero bicycle parking spaces per townhouse dwelling unit is consistent with the approach and requirements for “street townhouse” dwellings. Although the Proposed Development contemplates “cluster townhouses”, which require 10% of required vehicle parking as bicycle parking, “street townhouse” dwellings are not permitted to provide bicycle parking. This is largely due to the provision of integrated garages in street townhouse dwellings, which offers a secure place for bicycles. Each cluster townhouse proposed includes a garage, meeting the intent of bicycle parking for this built form.

The inclusion of the maximum FSI of the Phase 1 lands is to provide clarity on this provision due to the multiple built forms across one property. The 1.0 FSI is in keeping with the as-of-right permission for apartment dwellings in the R5 zone. Phase 1 of the Proposed Development has an FSI of roughly 0.75 (net of the proposed collector road), so while the amendment may not be required, it is proposed out of an abundance of caution so ensure Phase 1 can be implemented as proposed.

The Subject ZBA proposes a reduction to the parking ratio for apartment buildings of Phase 2 to 1.0 spaces per unit from 1.25 spaces per unit. The two nearby bus routes, nearby cycling facilities, and significant commercial spaces within walking distance of the Phase 2 portion of the Subject Site reduce the need for residential parking. Further, the reduction in surface parking aligns with planning policy to prepare for the impacts of a changing climate, reduce greenhouse gas emissions, and improving air quality. By reducing parking, the need for single-occupant vehicles will be decreased which is consistent with the expected future trend of reduced reliance on single-occupant vehicles anticipated to be present at the time of build-out and occupancy of Phase 2. Additionally, it is noted that Phase 2 is a preliminary concept at this time.

The Subject ZBA and Proposed Development are in keeping with the intent of the R5 zone, implementing the high-density residential land use designation proposed for the Subject Site.

## 8. Summary and Conclusion

This Planning Report has been prepared in support of the submission of the Official Plan and Zoning By-law Amendment and Site Plan Approval applications for the lands known municipally as 1555 18<sup>th</sup> Avenue East in Owen Sound to facilitate redevelopment of the property. The Official Plan Amendment proposes to redesignate the Subject Site to the High-Density land use designation in the Sydenham Heights Planning Area. The Zoning By-law Amendment Application proposes to rezone the Subject Site to an R5 exception zone.

The Subject Site is within the County's Primary Settlement Area, which is planned to accommodate residential development. It is within walking distance of transit and a short distance from the Grey County CP Rail Trail. The Subject Site is adjacent to a significant commercial node and is a short distance from the downtown area.

The Proposed Development contains two phases. The first consists of two, four storey buildings, and 87 townhouses units located in 10 blocks, and the second contemplates three, four storey buildings. Parking for Phase 1 is provided in a surface parking lot and at each townhouse unit, with Phase 2 parking being provided in surface parking lots between each building. This Report concludes the Official Plan and Zoning By-law Amendment Applications for the property is justified for the following reasons:

- They are consistent with the PPS;
- They conform to the County OP;
- They meet the intent of the City OP and Sydenham Heights Planning Area;
- They meet the intent of the Zoning By-law; and,
- The Proposed Development will efficiently use available municipal infrastructure.

It is our opinion that the Subject Applications are in the public interest, represent good planning, and should be approved.

# Appendix A - Draft Official Plan Amendment



THE CORPORATION OF THE CITY OF OWEN SOUND

BY-LAW NO. \_\_\_\_\_

A By-law to adopt Amendment No. \_\_\_\_ to the Official Plan for the City of Owen Sound.

The Council of the Corporation of the City of Owen Sound, pursuant to the provisions of the Planning Act, R.S.O. 1990, as amended, does hereby enact as follows:

1. THAT Amendment Number \_\_\_\_ to the Official Plan for the City of Owen Sound, consisting of the attached maps and explanatory text, is hereby adopted.
2. THAT this By-law shall come into force and take effect on the day of the final passing thereof.

READ A FIRST AND SECOND TIME THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_

READ A THIRD TIME AND PASSED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_

\_\_\_\_\_  
MAYOR

\_\_\_\_\_  
CLERK

**AMENDMENT NUMBER \_\_\_\_ TO THE  
CITY OF OWEN SOUND OFFICIAL PLAN**

**INDEX**

**PART A - THE PREAMBLE**

The Preamble provides an explanation of the proposed Amendment including the purpose, location, and background information, but does not form part of this Amendment.

**PART B - THE AMENDMENT**

The Amendment describes the modifications to the City of Owen Sound which constitute Official Plan Amendment Number \_\_\_\_.

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## **PART A – THE PREAMBLE**

The details of the amendment, as contained in Part B of this text, constitutes Amendment No. \_\_\_\_ to the City of Owen Sound Official Plan.

### **LOCATION**

The land affected by this amendment are southern area of the property municipally known as 1555 18<sup>th</sup> Avenue East, legally described as RANGE 5 EGR PT PK LOTS 7 8;16R5510 PT 1 16R7308 PTS 7;TO 14 16R7779 PT PT 7 23 PTS;3 4 5 8 9 13 15 TO 18 20 22Part, as demonstrated on Schedule A, affixed.

### **PURPOSE AND EFFECT**

The purpose of this amendment is to revise the land use designation of the affected land on schedule A2 to the “High Density Residential” land use designation and to revise policy 4.2.5.3 to facilitate an alternative collector road configuration through the affected land.

### **BASIS**

The basis for permitting this amendment is as follows:

- The proposed amendment is consistent with the Provincial Policy Statement.
- The proposed amendment is in conformity with the County of Grey Official Plan.
- The proposed amendment is in keeping with the policies of the City of Owen Sound Official Plan.
- The amendment will make efficient use of underutilized land within a Primary Settlement Area.

## **PART B – THE AMENDMENT**

### **DETAILS OF THE AMENDMENT**

The Official Plan of the City of Owen Sound is hereby amended as follows:

#### **TEXT CHANGES**

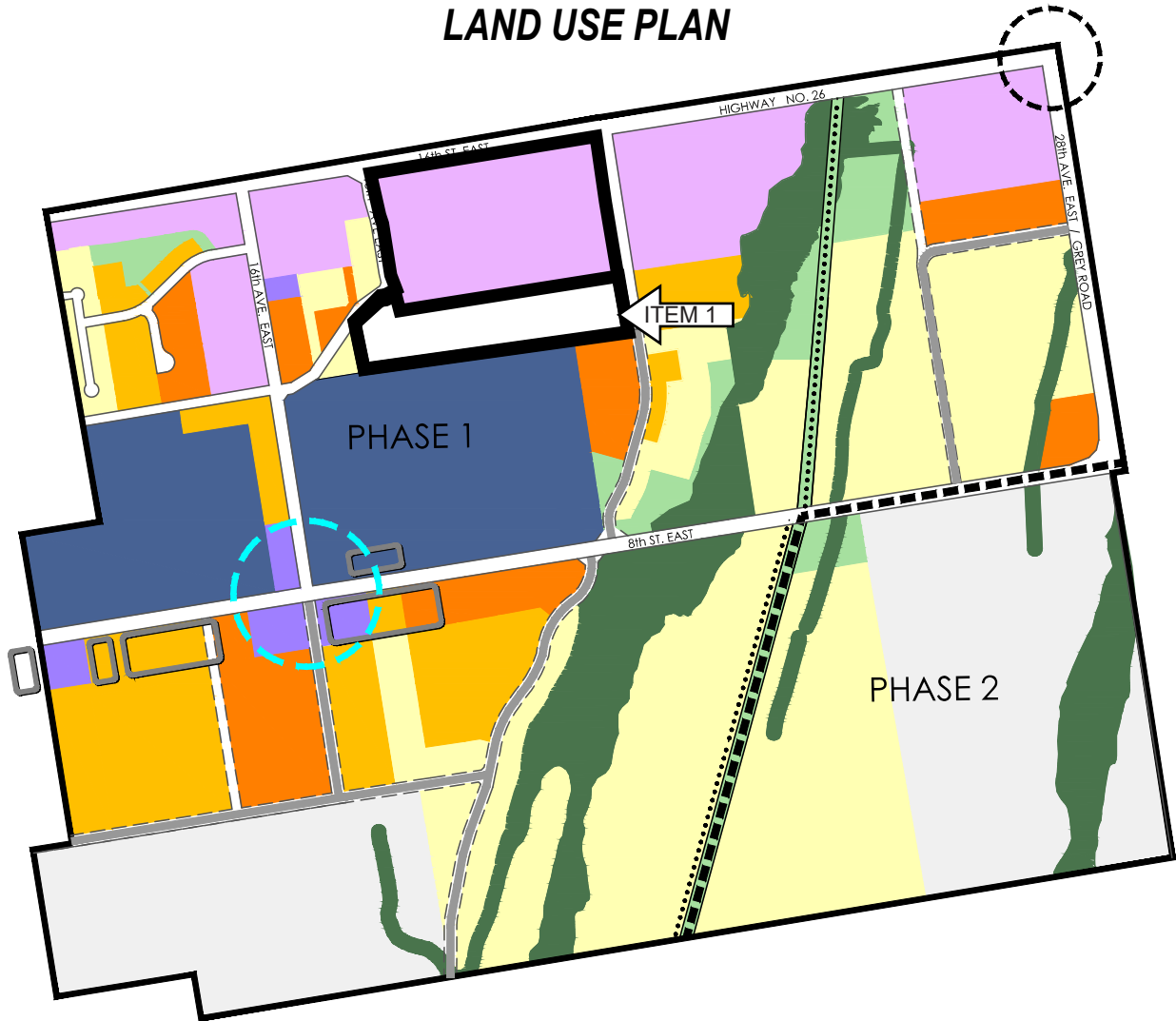
Policy 4.2.5.3 is amended to include:

*c) A third collector road will be the extension of 10<sup>th</sup> Street East from 18<sup>th</sup> Avenue East to 20<sup>th</sup> Avenue East. This collector road may be 20.5 metres in width.*

#### **MAP CHANGES**

The affected lands are to be redesignated “High Density Residential” on Schedule A2, as demonstrated on Schedule A, affixed.

SCHEDULE "A"  
 AMENDMENT NO. \_\_\_\_\_  
 TO THE  
**OWEN SOUND OFFICIAL PLAN**  
 SCHEDULE "A"  
**OWEN SOUND  
 LAND USE PLAN**



**- AREA OF THIS AMENDMENT**

ITEM 1- LANDS TO BE REDESIGNATED TO HIGH DENSITY RESIDENTIAL



- |                              |   |
|------------------------------|---|
| Planning Area Boundary       | Potential Location of Easthill Elevated Water Storage |
| Low Density Residential      | Future Collector Roads                                |
| Medium Density Residential   | Future Roads  |
| High Density Residential     | Trail   |
| East City Commercial         | Phase Boundary  |
| Arterial Commercial          | Future Mixed Use Node                                 |
| Institutional                | Gateway   |
| Open Space                   |   |
| Hazard Land                  |   |
| Niagara Escarpment Plan Area |   |

## **Appendix B - Draft Zoning By-law Amendment**

# The Corporation of the City of Owen Sound

Zoning By-law Amendment No. \_\_\_\_

**Being a by-law to amend Zoning By-law 2010-078 of the City of Owen Sound pertaining to RANGE 5 EGR PT PK LOTS 7 8;16R5510 PT 1 16R7308 PTS 7;TO 14 16R7779 PT PT 7 23 PTS;3 4 5 8 9 13 15 TO 18 20 22**

**Whereas**, the Council of the Corporation of the City of Owen Sound deems it in the public interest to pass a by-law to amend By-law 2010-078; and,

**Whereas**, pursuant to the provisions of Sections 34 & 36 of the Planning Act R.S.O. 1990, as amended, by-laws may be amended by Councils of Municipalities.

**The Council of The Corporation of the City of Owen Sound enacts as follows:**

1. By-law 2010-078 is hereby amended by re-zoning the lands shown on "Schedule A", affixed hereto, to the R5 14.XX and R5 14.YY Zones.
2. Section 14 of By-law 2010-078 is hereby amended by adding the following:

*Notwithstanding the provisions of the General Residential (R5) Zone and for lands shown on Schedule A, Zoning Map 20, in addition to all uses permitted in the R5 zone the following shall be permitted.*

- i) *A Temporary Sales/Leasing Office defined as "a building or structure, facility or trailer on the lot used for the purpose of the sale or lease of dwelling units to be erected on the lot and/or the administration and management of construction activity related to the construction on the lot."*

*Notwithstanding the provisions of the R5 Zone, the following regulations shall apply:*

- i) *The lots subject to this Special Provision shall be regarded as one lot for the purposes of meeting zoning regulations.*
- ii) *Severances and lot divisions by way of consent are permitted without meeting minimum lot frontage, setbacks, lot coverage and area requirements to newly created lot lines and for newly created parcels, as these lands are considered one lot for planning purposes.*
- iii) *Parking, loading, and driveways providing access to parking shall not require a setback from interior property lines shared with properties zoned R5 14.XX or R5 14.YY on By-law 2010-078 Zoning Map 20.*
- iv) *For lands zoned R5 14.XX or R5 14.YY, maximum building height shall be 15 metres.*
- v) *For lands zoned R5 14.XX, the minimum exterior side yard shall be 1.5 metres.*
- vi) *For lands zoned R5 14.XX, 0 bicycle parking spaces per unit are required for cluster townhouse dwellings where a garage is provided.*
- vii) *For lands zoned R5 14.XX, the maximum FSI is 1.0.*
- viii) *For lands zoned R5 14.YY, a parking ratio of 1.0 spaces per unit is permitted for apartment dwellings.*

3. Schedule "A" and all notations thereon, are hereby declared to form part of this By-law.
4. This by-law shall come into force and take effect upon being passed by Council, pursuant to the Planning Act, R.S.O. 1990, as amended.

**Read a first, second and third time and finally passed this \_\_\_ day of \_\_\_\_\_, 20\_\_.**

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**Mayor**

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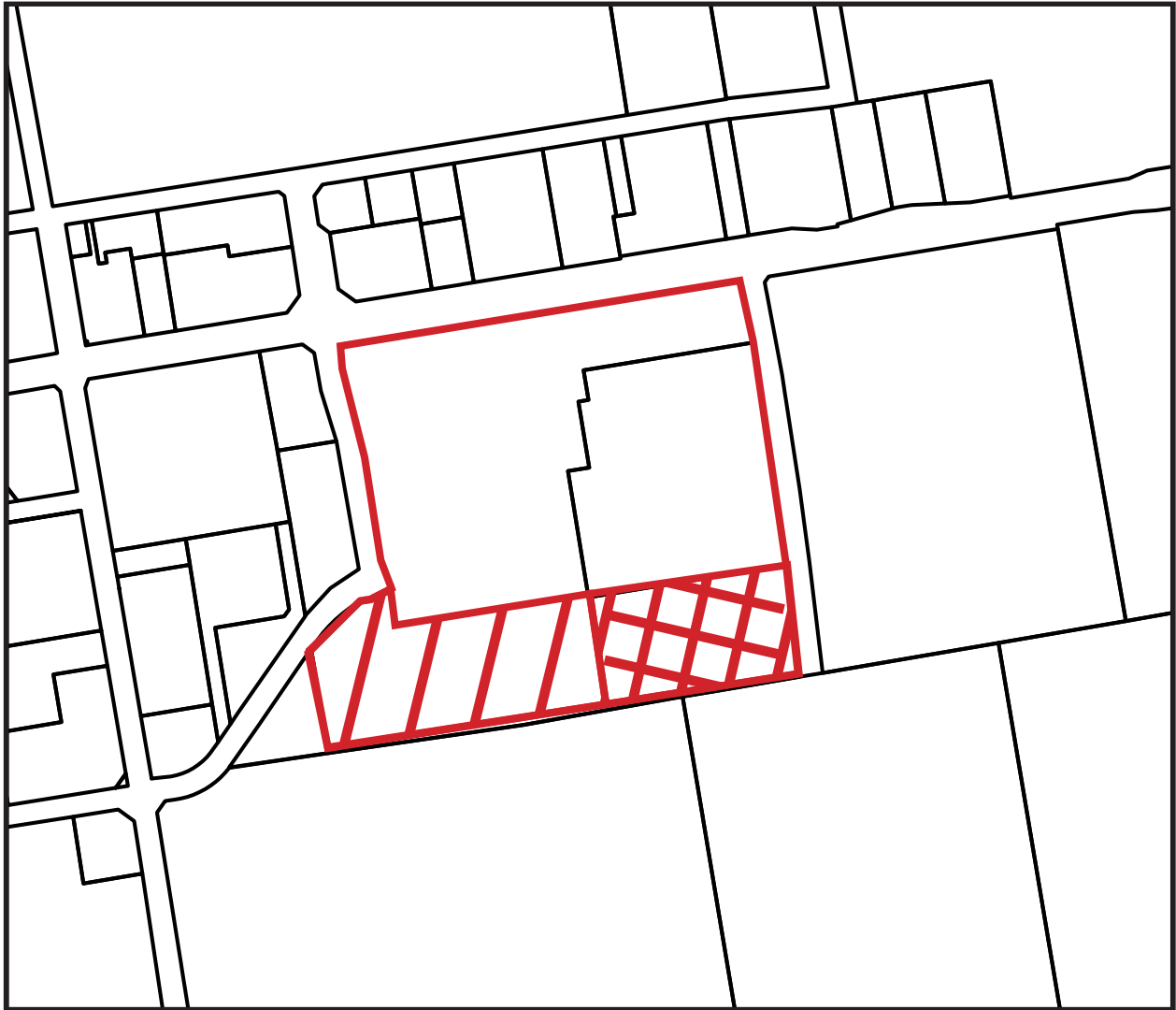
**Clerk**

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# CITY OF OWEN SOUND ZONING BY-LAW AMENDMENT

## SCHEDULE 'A'



 Land to be Rezoned R5(14.XX)

 Land to be Rezoned R5(14.YY)