



URBAN DESIGN REPORT

1555 18TH AVENUE EAST

Calloway Real Estate Investment Trust Inc.

City of Owen Sound

Official Plan Amendment/Zoning Bylaw Amendment

Site Plan Approval

January, 2023

Georgian Bay

Grey Road 15

20th St E

16th Ave E

18th Ave E

20th St E

SITE

10th St E

16th Ave E

8th St E



TABLE OF CONTENTS

1. BACKGROUND AND PURPOSE.....	4	5. RESPONSE TO POLICY AND GUIDELINE FRAMEWORK	24
1.1 Scope	4	5.1 Natural Features (Section 8.2)	24
1.2 Proposed Development	4	5.2 Experiencing the City (Section 8.3).....	24
1.3 Report Content	4	5.2 Pedestrian Environment (Section 8.4)	25
1.4 Supporting Studies and Materials	4	5.3 Streetscape (Section 8.6).....	26
2. EXISTING SITE CONDITIONS AND CONTEXT	6	5.4 Safety and Security (Section 8.7).....	28
2.1 Location and Description	6	6. SUMMARY	29
2.2 Existing Site Condition and Vegetation	6		
2.3 Surrounding Land Uses and its Compatibility	8		
2.4 Transportation Network.....	10		
3. DESIGN POLICY CONTEXT	11		
4. PROPOSED DEVELOPMENT	12		
4.0 Development Vision and Concept Plan	12		
4.1 Built Form	14		
4.1.1 Apartment Buildings	14		
4.1.1.1 Buildings Articulations and Materials.....	14		
4.1.2 Townhouses	17		
4.1.2.1 Buildings Articulations and Materials	17		
4.3 Access and Circulation.....	18		
4.4 Garbage and Loading Access	20		
4.5 Parking	20		
4.6 Lighting and Signage	20		
4.7 Landscape and Amenity Area	22		

1. BACKGROUND AND PURPOSE

1.1 Scope

GSP Group has been retained by SmartCentres on behalf of Calloway REIT Inc. (the “Client”) to prepare an Urban Design Brief for the redevelopment of the property municipally addressed as 1555 18th Avenue East in the City of Owen Sound (the “Subject Site”). The purpose of the Urban Design Brief is to illustrate how the proposal has sought to facilitate the comprehensive redevelopment of the Subject Site to include a multi-phase residential development of varying heights and densities in accordance with the development goals of the City of Owen Sound. The Proposed Development will include the following:

- **Phase 1** consists of two apartment buildings with a total of 156 units with 0.087 ha private amenity area and 195 surface parking spaces, and a total of 87 townhouse units provided in ten blocks with two parking spaces per dwelling units.
- **Phase 2** will be an extension of Phase 1 and will consist of three apartment buildings with a total of 234 units, and surface parking spaces grouped between buildings. Phase 2 is in concept design stage and detailed design elements will be provided at a future site plan process.

A 20.5-metre-wide modified Collector Road is proposed across the entire site to connect the extension of 10th Street East and 18th Avenue East to 20th Street East.

1.2 Purpose

An Official Plan Amendment (“OPA”) and Zoning By-law Amendment (“ZBA”) is required in order to facilitate the Proposed Development. An Urban Design Brief was identified as a requirement of these applications as per the pre-submission consultation meeting dated February 24th, 2022.

A Site Plan Approval (“SPA”) application is also being submitted as part of the application package. The Proposed Development will be implemented

through two phases. The SPA application pertains to only Phase 1, as further detailed in Section 4 of this report.

1.3 Report Content

Based on the matters for consideration and evaluation identified in the pre-submission consultation record, this Urban Design Brief:

- Describes the existing physical conditions on the Subject Site and the existing neighbourhood context surrounding the Subject Site (Section 2);
- Describes the Proposed Development (Section 4);
- Summarizes the principal City policy and guideline documents applicable to the subject Site and the relevant sections as they relate to the Proposed Development (Section 5); and
- Makes conclusions regarding the findings (Section 6).

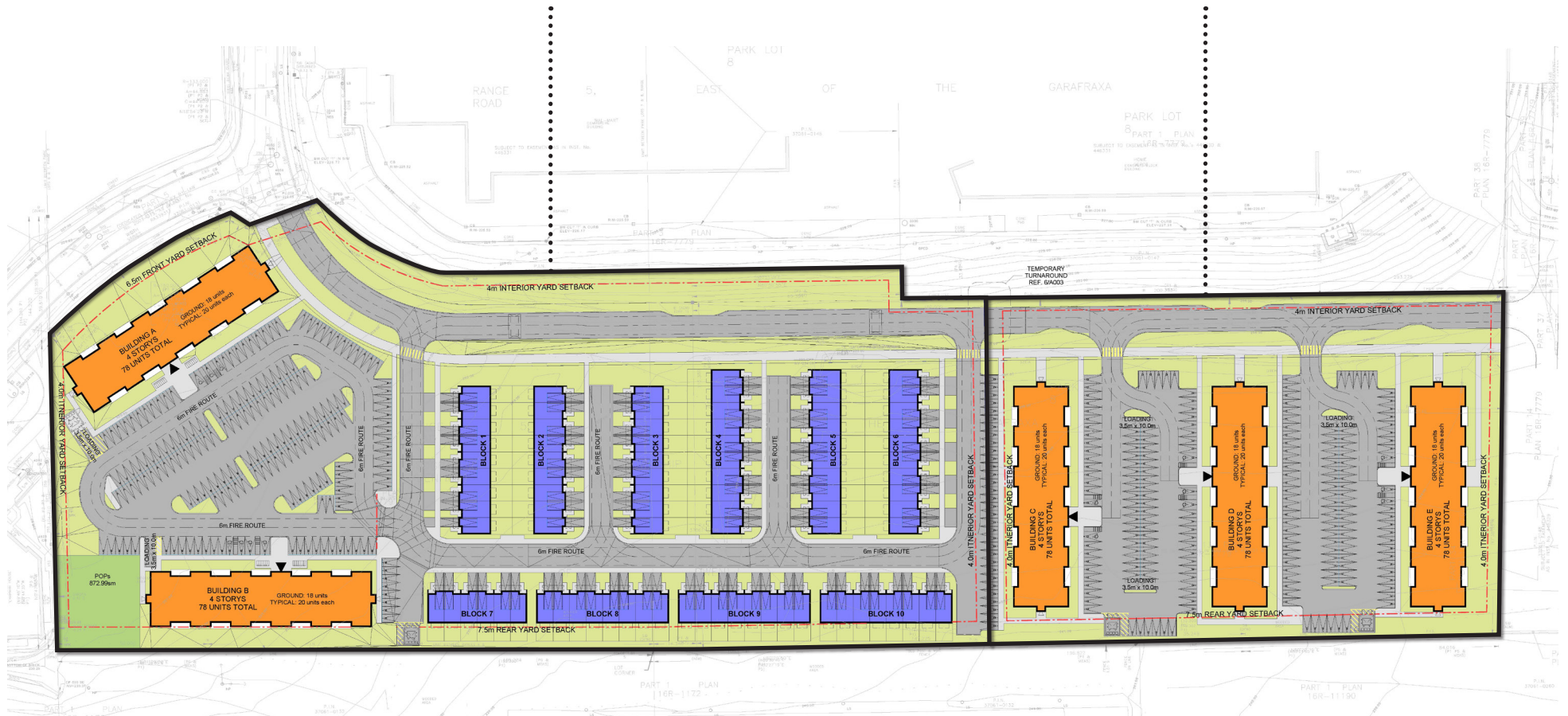
1.4 Supporting Studies and Materials

This Urban Design Report has considered the following plans and reports prepared in support of the subject applications:

- Site Plan prepared by Chamberlain Architect Ltd.;
- Floor Plans and Elevations prepared by Chamberlain Architect Ltd.; and
- Landscape Concept Plan prepared by MHBC ;

PHASE 1

PHASE 2



Overall Site Plan (Phase 1 & 2), prepared by Chamberlain Architect Ltd. (January, 2023)

2. EXISTING SITE CONDITIONS AND CONTEXT

2.1 Location and Description

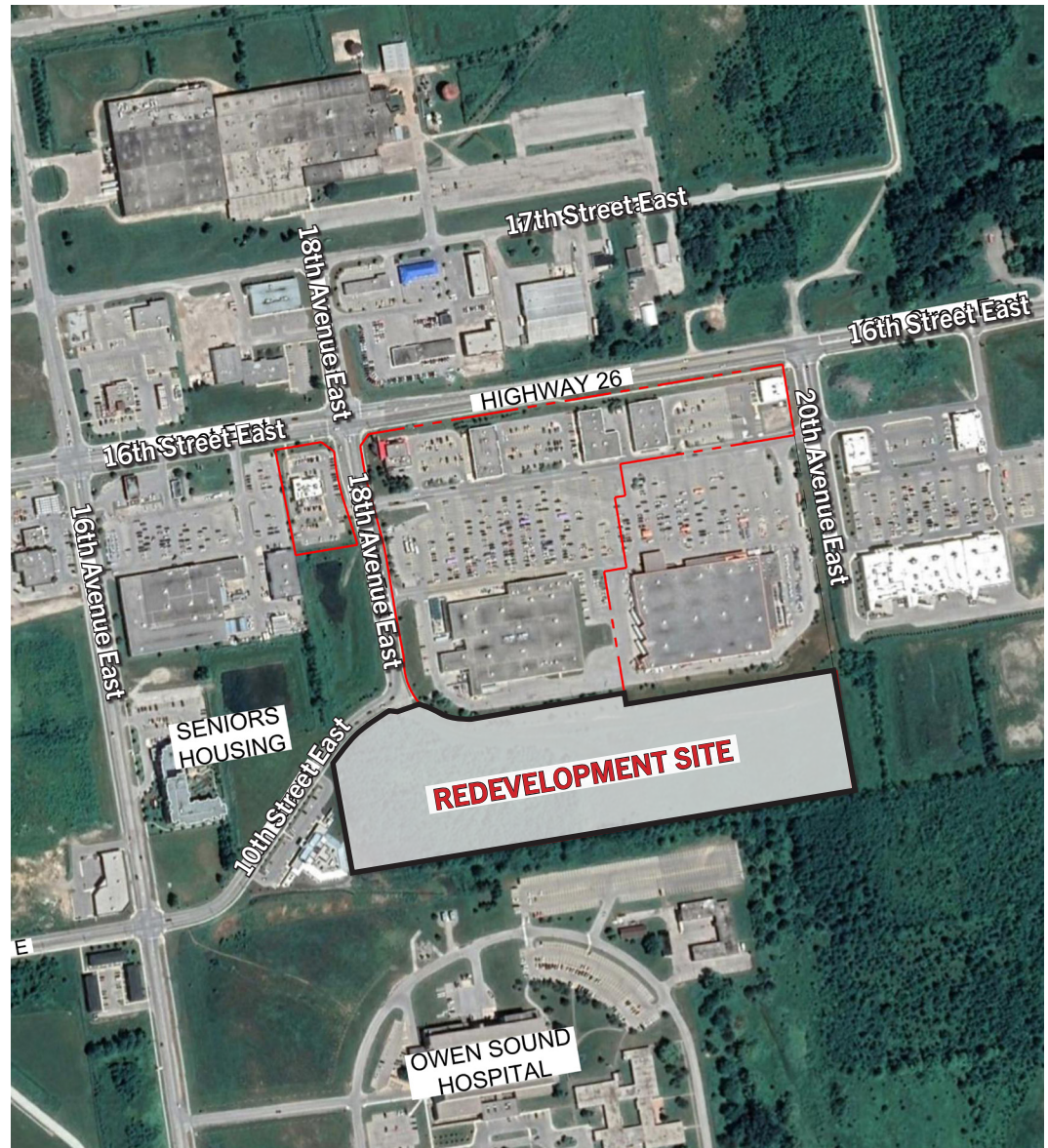
The Subject Site is situated in the southern portion of the SmartCentres site generally bounded by 10th Street East and Grey Bruce Hospice Centre to the west, SmartCentres site to the north, 20th Avenue East to the West, and Owen Sound Hospital to the south. The Subject Site to which the Official Plan Amendment, Zoning By-law Amendment and Site Plan Approval applications (the "Subject Applications") apply is approximately 6 hectares in size. The Subject Site has vehicular access from 10th Street East and 18th Avenue East.

2.2 Existing Site Condition and Vegetation

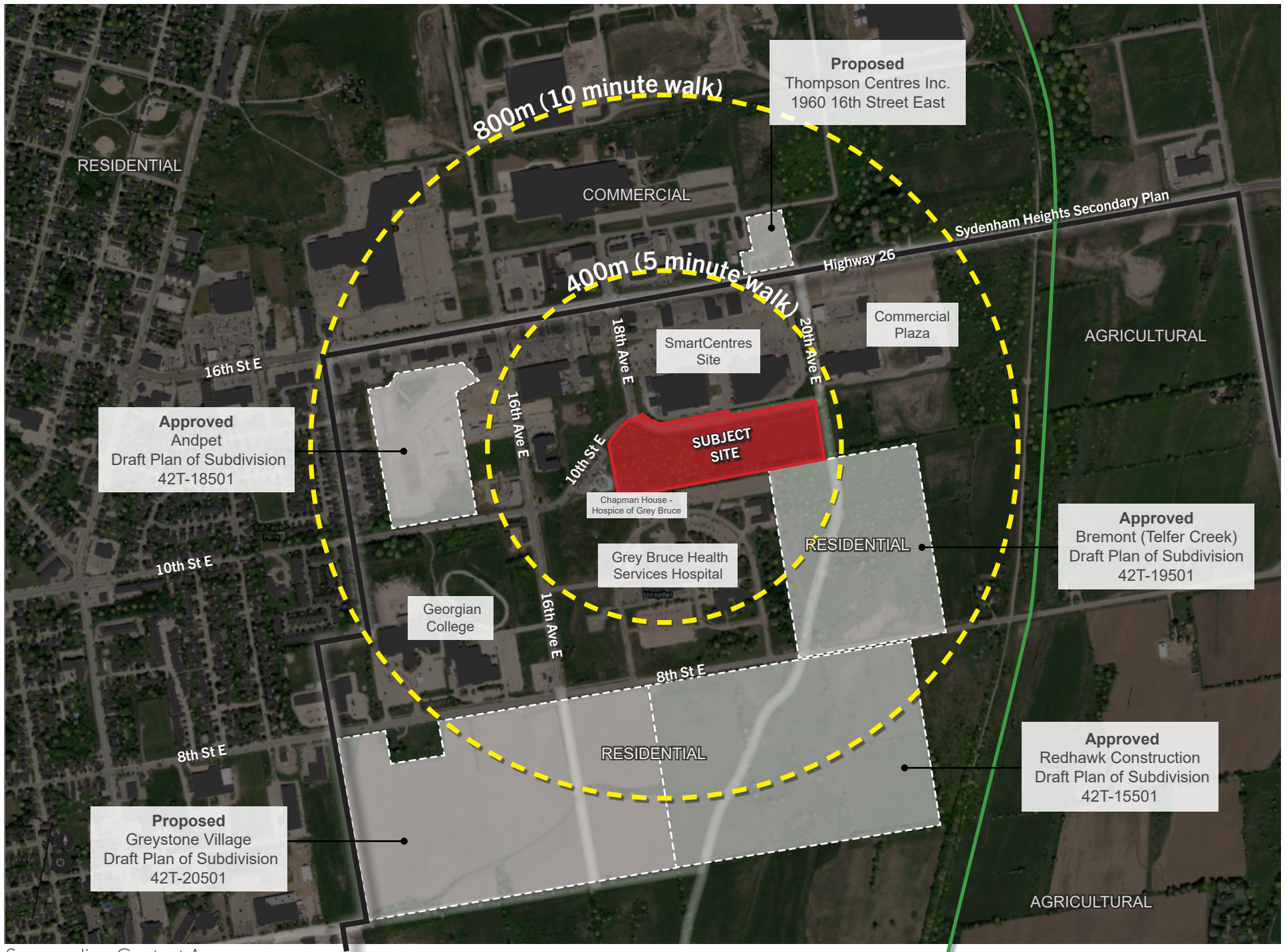
There are no existing buildings or structures on the Subject Site. The Subject Site contains deciduous forest stand area and an open field, and few landscaping trees exist along the northern perimeter of the Subject Site.

The Tree Management Plan was prepared by Kuntz Forestry Consulting Inc. for Phase 1 of the Proposed Development. A tree inventory of the Subject Site documented 106 trees on and within six metres of the property. The majority of the deciduous forest stand area is in dead and/or hazardous conditions and the removal of 76 trees is also required, given the necessary grading and construction works associated with the Proposed Development. The remaining 30 trees can be saved provided appropriate tree protection measures are installed prior to the development. New plantings as part of the development would address the removals.

The Subject Site sits on a much higher elevation in comparison to the SmartCentres site (to the north) with a noticeable grade change from the highest points on the property to lowest points to the back of the Walmart building. The public sidewalk along 10th Street East sits at grade with the Subject Site.



Study Area



Surrounding Context Area

2.3 Surrounding Land Uses and its Compatibility

The Subject Site is located in the eastern part of Owen Sound's city centre and within Phase 1 of the Sydenham Heights Planning Area. The Sydenham Heights Planning Area is proposed to be largely a residential mixed-use community. The Sydenham Heights community is located in the south eastern portion of the City. While it is located within the urban area, it has yet to be fully developed. Presently, the Sydenham Heights community is distinguished by existing major institutional uses, commercial uses, a mixed-use node at 16th Avenue East and 8th Street East and a mix of new residential subdivisions interspersed with remnant agricultural fields and natural heritage/open spaces areas. The Subject Site provides easy access to the city centre and amenities as it includes transit stops along 10th Street East that runs to the west and north parts of the City. There is a school, a college, a hospital, and shopping centres within 5-10 minute walking distance and easily accessible by active transportation modes.

To the immediate north of the Subject Site, is the East City Commercial Area (as shown on the Sydenham Heights Planning Area, Schedule A2 Land Use), which is currently occupied by commercial uses on the north and south sides of 16th Street East. The East City Commercial Area provides a range of retail and service commercial uses in the form of large retail buildings and smaller stand-alone retail buildings including the SmartCentres commercial plaza. The SmartCentres is an unenclosed shopping centre located on the southeast corner of 16th Street East and 18th Avenue East, occupied by retail and service commercial uses in blocks consisting of smaller stand-alone retail buildings and large format retail outlets including the Walmart (1555 18th Avenue East) and the Home Depot (1590 20th Avenue East). The back side of the Walmart building and the Home Depot building abuts the Subject Site with a noticeable upward slope in a 3:1 ratio resulting in the Subject Site sitting at a higher elevation in comparison to the SmartCentres site. This topographical difference along this shared property line could be seen as an advantage as it serves as a privacy screen in conjunction with the landscaped buffer area and mitigates noise concerns.

The property to the east side of the Subject Site across 20th Avenue East is currently a vacant land. The south side of the Subject Site is the Grey Bruce Health Services Hospital (1800 8th Street East). The hospital block is

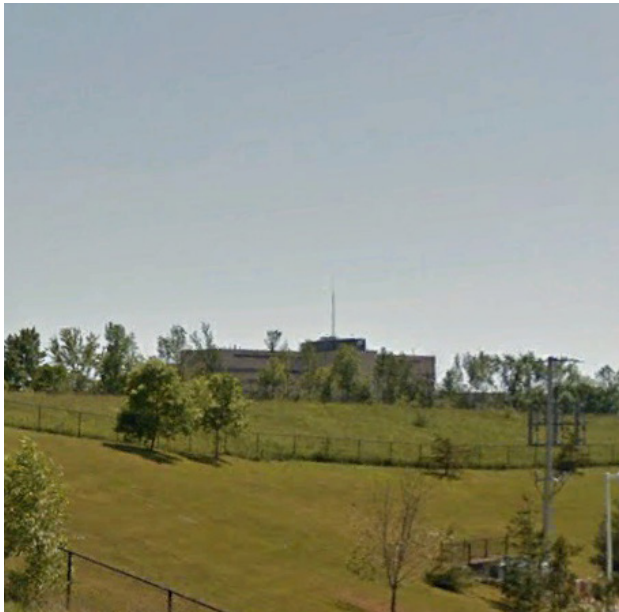
a large site that is generally bounded by 16th Avenue East to the west and 8th Street to the south. The hospital building is setback approximately 145 metres from its property line which provides more than sufficient separation distance from the Proposed Development. To the immediate east of Grey Bruce Health Services Hospital are currently vacant agricultural properties with a tributary of Bothwell's Creek that is located along the southeast corner of the lands. These lands are part of the Telfer Creek (Bremont) (42T-19501) residential subdivision approved for 328 residential units in the form of single detached, semi-detached, townhouse units and medium/ high density residential/commercial buildings. The approved draft plan of subdivision includes the future extension of 20th Avenue East that bisects the lands and provides the main access to the subdivision from 8th Street East. The plan also includes parkland blocks, stormwater management facilities as well as hazard lands containing a tributary to Bothwell's Creek. Further east, along 8th Street East, are rural residential lands.

Chapman House – Residential Hospice of Grey Bruce is located west along 10th Street East west of 18th Avenue East (1725 10th Street East). The proposed 4-storey buildings (Buildings 1 & 2) are set back at 22 and 30 metres from its shared western property line, adjacent to Chapman House. The intervening spaces between the proposed 4-storey buildings and shared property line will consist of an open landscape area and a driveway with one-sided parking spaces.

The broader and future planned context of the site and the surrounding area foresees urbanization, intensification, and compact developments. Planning applications for a residential subdivision have been approved for the south side of 8th Street East. The lands are part of Redhawk Construction Ltd. residential subdivision (42T15501) between 16th Avenue East and the future extension of 20th Avenue East for development of 366 units in the form of single detached dwellings, townhouse units, and multi-unit residential buildings. The plan of subdivision includes a north/south extension of the future 20th Avenue East that will intersect with 8th Street East to the north and terminate in a cul-de-sac at the south end of the property. There is an east-west road that is planned to terminate in a cul-de-sac but could connect to the Subject Site to the west as part of the extension of 6th Street East.



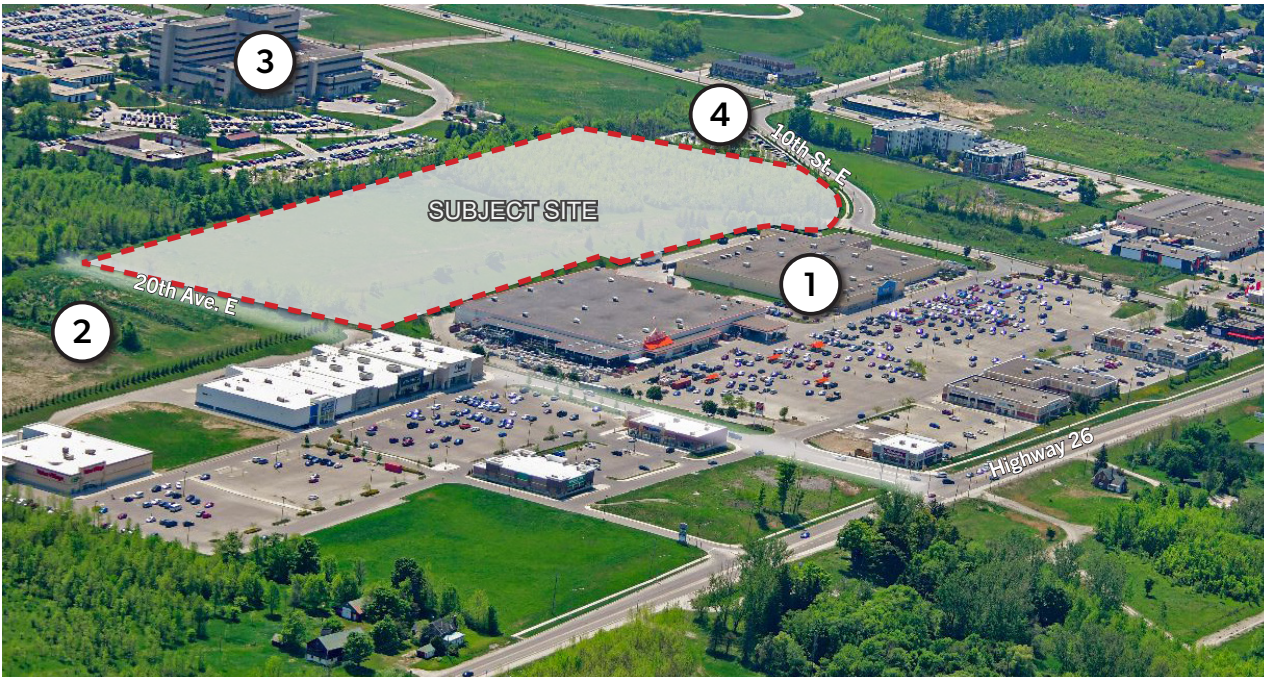
① SmartCentres Shopping Centre



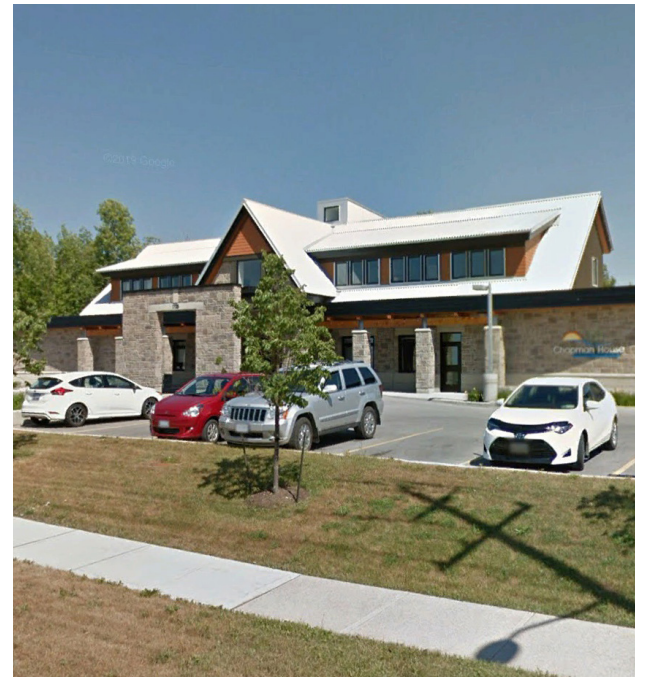
② Future Phase 2 of the Proposed Development



③ Grey Bruce Health Services Hospital (1800 8th Street East)



Immediate Site Context



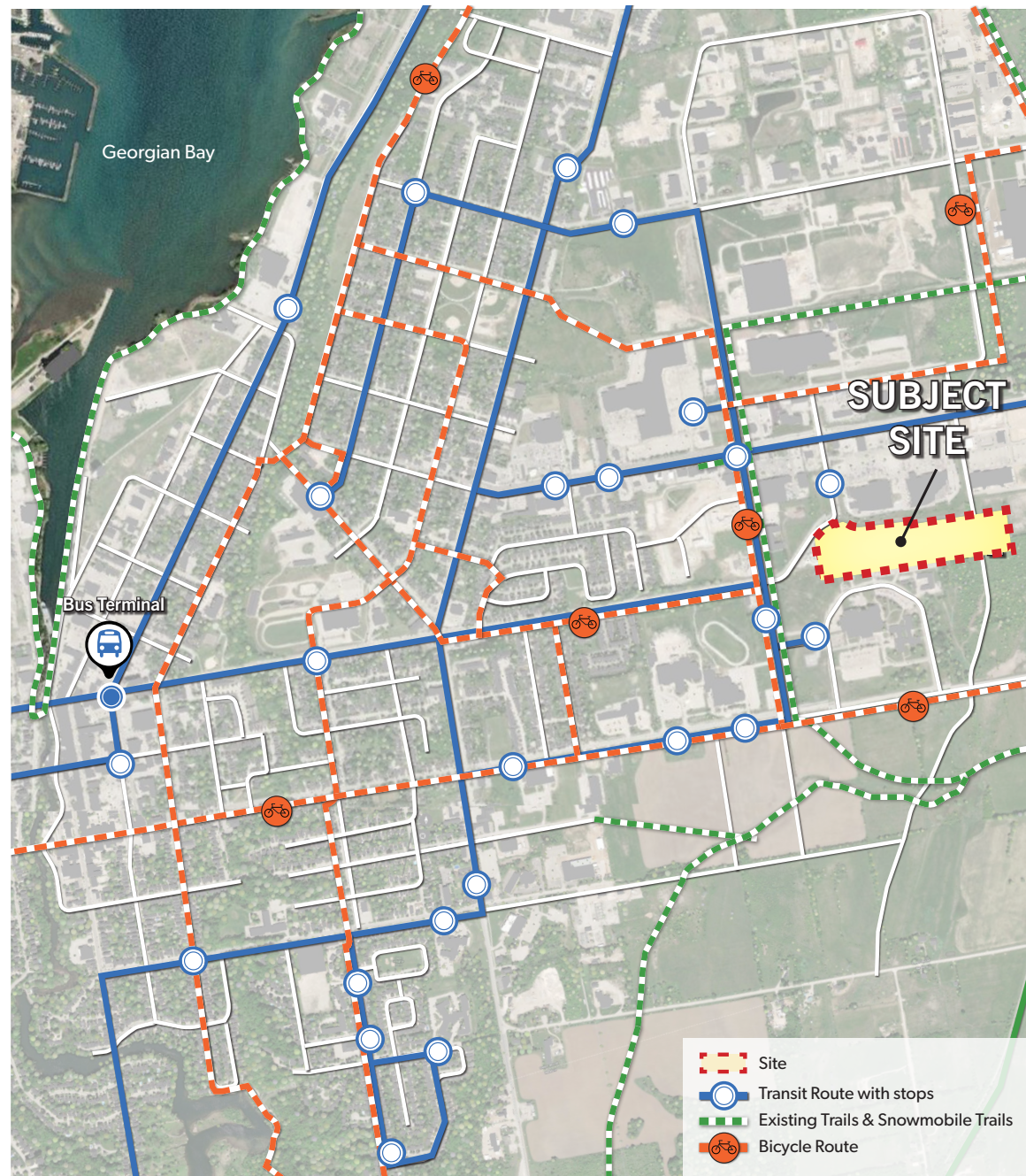
④ Chapman House - Hospice of Grey Bruce

2.4 Transportation Network

The Subject Site is located in an area of the City that is connected to transit, recreational trails, and has convenient road access to the greater City. The Subject Site is located where 18th Avenue East meets 10th Street East, both of which are classified as Collector Roads in the Official Plan. These roads provide one travel lane in each direction, providing direct access to downtown Owen Sound as well as the greater commercial area surrounding the Subject Site. A Transportation Impact Study ("TIS") was undertaken by CGH Transportation, analyzing the existing and future traffic impacts related to the Proposed Development. The TIS finds that intersections in the Study Area are expected to operate with good overall line of sight and delay. The TIS further recommends a separated eastbound left turn lane at the intersection of 10th Street East and 18th Avenue East as a mitigation measure.

The Grey County CP Rail Trail is located near the Subject Site. This multi-use trail is 77 kilometres long, beginning near the Harry Lumley Bayshore Community Centre to the south of the Subject Site. A trail connection is located to the east of the Subject Site along 16th Street East. The trail follows a wide arc east of the city and runs parallel to Highway 6 to Chatsworth. In addition to this trail, Schedule D of the Owen Sound Official Plan designates 10th Street East, 8th Street East, and 16th Avenue East as bicycle routes.

The Subject Site is in proximity to public transit. The East Bayshore bus route operates along 18th Street East and 8th Street East, where a stop is located within walking distance of the Subject Site. The bus route provides access to the major commercial centre along 16th Street East, hospital, and continues toward the downtown area of Owen Sound and the central transit terminal. The Core bus route operates along 16th Avenue East and 10th Street East, where a stop is again located within walking distance of the Subject Site.



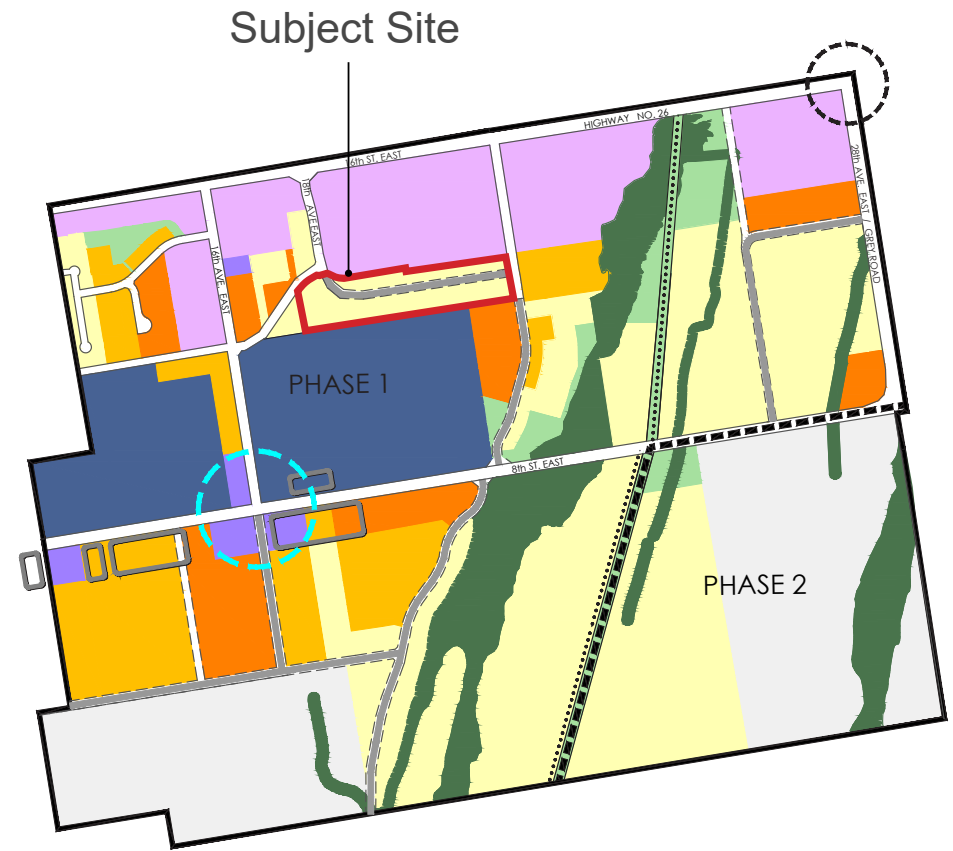
3. DESIGN POLICY CONTEXT

The Subject Site is designated “Residential” in the City of Owen Sound Official Plan (the “OP”), which permits a range of residential uses including those included in the Proposed Development. The Subject Site is within the Sydenham Heights Planning Area Phase 1. The Sydenham Heights Planning Area is located in the southeastern part of the City and has been intended and planned for development since being incorporated within the City boundary.

The Subject Site is designated “Low Density Residential” in the Sydenham Heights Planning Area, Schedule A2 Land Use. The current zoning is R3 and R3(H) 14.5, which permits single-detached, semi-detached, duplex, townhouse, converted dwellings, and accessory apartments.

As such, the Proposed Development requires Zoning By-law Amendment and Official Plan Amendment applications (the “ZBA”, “OPA” and “Subject Applications”). A Site Plan Approval application is also being submitted as part of the application package (“SPA”). The Proposed Development will be implemented through two phases. The SPA application pertains to only phase 1, as further detailed in Section 4 of this Report.

A pre-application consultation meeting was held with the City on February 24, 2022. The site concept submitted for this meeting included a redesignation of the noted future Collector Road to a private road. Since this meeting, the site concept has been revised and maintains the future Collector Road.



Legend

- | | | | |
|--|------------------------------|--|---|
| | Planning Area Boundary | | Potential Location of Easthill Elevated Water Storage |
| | Low Density Residential | | Future Collector Roads |
| | Medium Density Residential | | Future Roads |
| | High Density Residential | | Trail |
| | East City Commercial | | Phase Boundary |
| | Arterial Commercial | | Future Mixed Use Node |
| | Institutional | | Gateway |
| | Open Space | | |
| | Hazard Land | | |
| | Niagara Escarpment Plan Area | | |

City of Owen Sound Official Plan Sydenham Heights Phase 1 and Phase 2

4. PROPOSED DEVELOPMENT

Design Vision and Concept Plan

Section 4 focuses on the design details of Phase 1 of the Proposed Development.

The Concept Plan reflects the vision of the Subject Site as a residential development with two four-storey apartment buildings and 10 townhouse blocks that features high quality design and engaging pedestrian realms. The Concept Plan consists of two four-storey apartment buildings containing 156 apartment units and a private amenity area located on the west side of the property and 87 townhouse units on the east side of the property. The private amenity area provided for the apartment buildings is 873 square metres (0.087 hectares), located at the southwest corner of the Subject Site.

The Concept Plan will provide an east-west connection from 10th Street East and 18th Avenue East to 20th Street East in the form of a 20.5-metre-wide modified Collector Road. The design of the modified Collector Road includes 3-metre-wide sidewalk along the developed portion of the Subject Site and traffic calming measures have been implemented to promote reduced vehicular speeds and minimize conflicts between road users.

The apartment buildings and townhouse blocks will be accessed via the private condominium roads with connections to the modified Collector Road. The condominium roads, although private in tenure, will be treated in a similar design manner as that of a public street with connected sidewalks and buildings fronting directly onto the roads. The private condominium roads have 1.5 metre concrete pedestrian sidewalks on one side of the road which provide connections to common amenity areas, surface parking area and sidewalks on public streets. The private road right-of-way will contain necessary utilities and street lighting and landscaping. The Concept Plan is planned to be developed incrementally in phases, generally starting from the west and incrementally building out to the east.

In summary, the Proposed Development has considered the surrounding land uses and has located the different building types and uses to ensure an appropriate transition to the existing land uses while providing improved connectivity through the Subject Site to the existing active transportation network. The Proposed Development represents a more compact built form that supports the efficient use of infrastructure and community services.

The following design objectives have been developed to inform the Concept Plan:

- To create a cohesive community development.
- To support walkability, active modes of transportation and transit.
- To provide human-scaled and street-oriented built form.
- To provide more housing choices to the community.
- To provide a positive pedestrian experience along street frontages through architectural design/articulation, enhanced landscaping, and appropriate transitions that promote social interaction, active streetscapes and a walkable community.
- To establish a safe, interconnected, and accessible pedestrian network throughout the Subject Site.
- To minimize the visual impact of utilities and services on the public realm within the Subject Site.
- To mitigate potential impacts from transportation and stationary noise.



Phase 1 - Site Plan, prepared by Chamberlain Architect Ltd. (January, 2023)

4.1 Built Form

4.1.1 Apartment Buildings

There are two apartment buildings, each building containing 78 dwelling units with a combined total of 156 dwelling units. Building A is positioned parallel to 10th Street East and Building B is positioned south of the Subject Site abutting Grey Bruce Hospital. The proposed apartment buildings are situated away from the easterly side to have appropriate buffering and separation from the townhouse development.

Building A setback 11.8 metres from the curbside edge to the side of the building and approximately 15.8 metres setback from the property line to building abutting 10th Street East. Building A will maintain ample exposure to the 10th Street frontage and generous setbacks from 10th Street East will allow sufficient space for landscaping to create an attractive pedestrian-scaled environment. Building B is setback 7.5 metres from the south property line and approximately 31.4 metres from the west property line, abutting the Grey Bruce Hospice Centre.

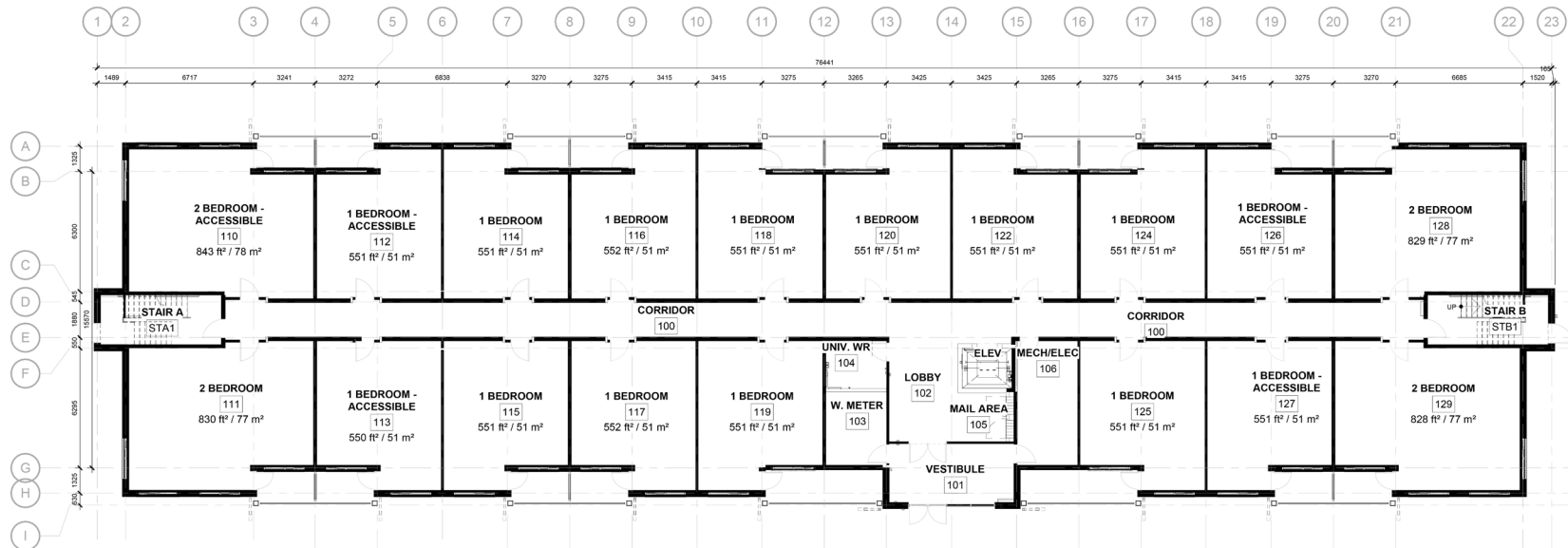
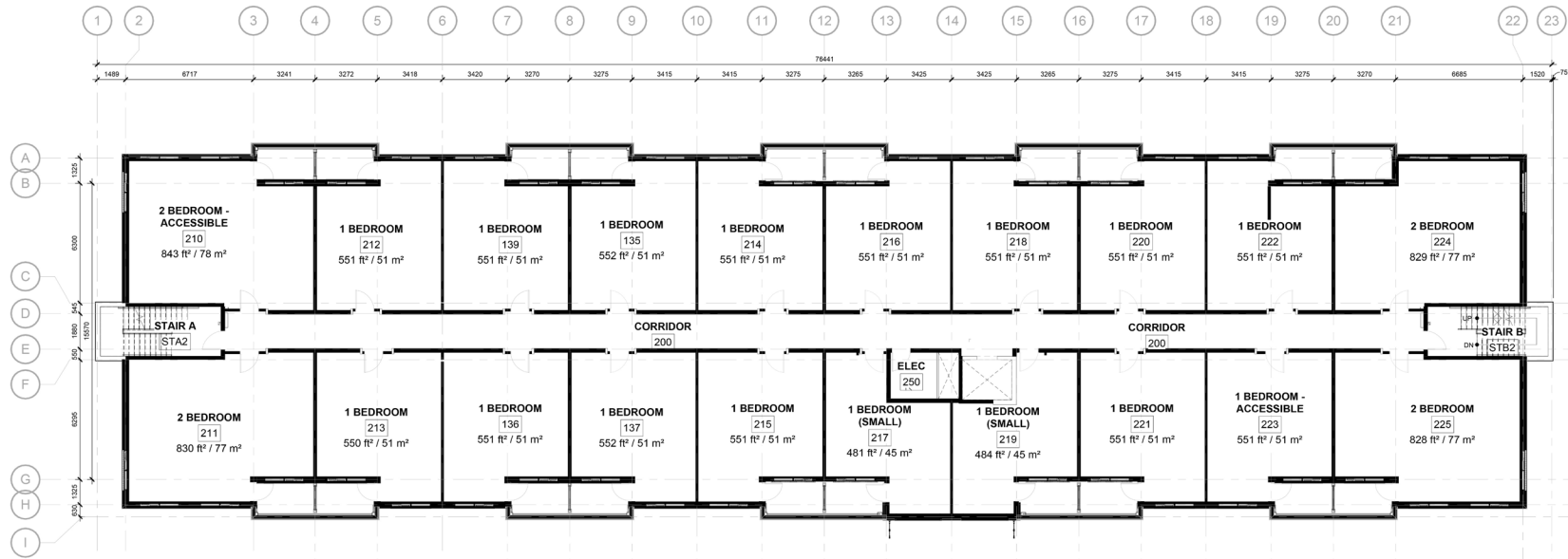
The proposed buildings are each four storeys (13.8 metres to top of the roof) in height. They are slab forms, with similar lengths and widths (approximately 76.4 metres long and 18.8 metres wide) and a combined building footprint of 2,593 square metres. The length of the buildings is broken down into smaller components through building articulation.

4.1.1.2 Buildings Articulation and Materials

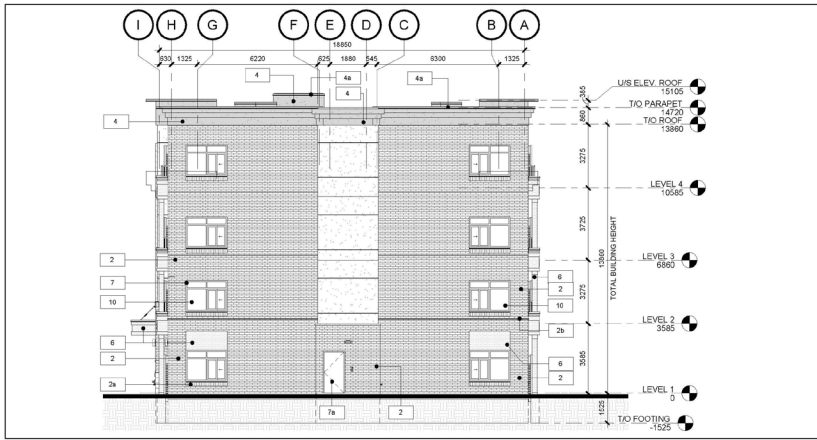
The articulation of the buildings, including the variation of materials, roofline, prominent building entrances and balconies are elements of aesthetic considerations. The proposed building materials, building shape, suite organization and site layout reflect considerations of cost efficiencies, functional programming, and productivity for occupants' wellbeing.

There are no blank facades; all elevations are detailed with glazing, articulation, and a mix of materials. Windows and openings are regularly spaced on all elevations. Balconies are proposed for all units in all buildings. Bays of paired balconies are spaced at regular intervals along the elevations, contributing to vertical articulation of the building, adding visual interest, and breaking down the apparent scale of the longer building elevations. Subtle architectural projections surround the balcony bays up to the top of the third storey.

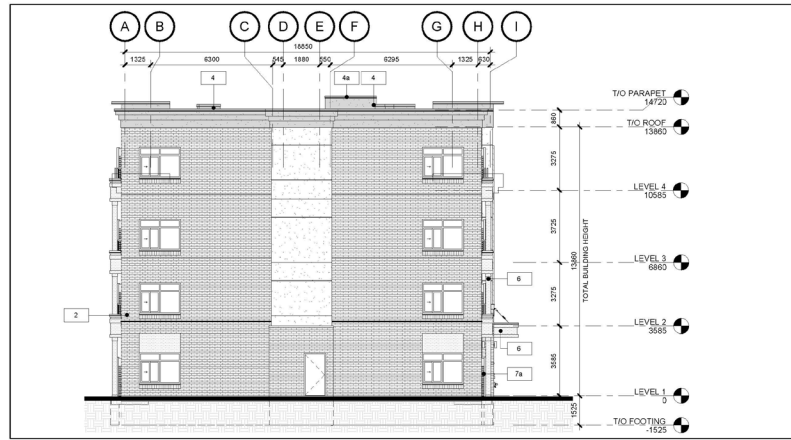
Vertical articulation is reinforced mid-building with differentiation in materials and proposing a continuous transparent glazing window wall from the second floor to the top of the third floor. Brick is used as the principal material, interjected with lighter coloured masonry cladding mid-building and sides of the building for accent and interest. Building entrances are emphasized with stone and brick cladding at grade and projecting canopies with integrated building signage. The roofline of the buildings follows this pattern with projecting parapets regularly spaced along the long facades.



Apartment Building Floor Plans, prepared by Chamberlain Architect Ltd. (January, 2023)



4 RIGHT ELEVATION
A115 1:125



3 LEFT ELEVATION
A115 1:125

MATERIAL LEGEND

1	MASONRY - STONE
2	MASONRY - BRICK
2a	MASONRY - BRICK (SOLDER COURSE)
3	EXTERIOR INSULATION FINISH SYSTEM (EIFS) - LIGHT
3a	EXTERIOR INSULATION FINISH SYSTEM (EIFS) - TRIM - LIGHT
4	EXTERIOR INSULATION FINISH SYSTEM (EIFS) - DARK
4a	PREFINISHED METAL FLASHING TO MATCH ADJACENT
5	PRECAST CONCRETE TRIM
6	CEMENT PANEL SIDING
6a	PREFINISHED METAL FLASHING TO MATCH ADJACENT
7	THERMALLY BROKEN VINYL WINDOW TRIM
7a	PREFINISHED METAL PAINTED TO MATCH WINDOW TRIM
8	COMPOSITE WOOD DECK
9	VENTED ALUMINUM SOFFIT
10	DOUBLE GLAZED PANEL SEE WINDOW SCHEDULE & NOTES
11	GLAZED SPANDREL PANEL SEE WINDOW SCHEDULE & NOTES

PRODUCT INFORMATION

- MASONRY - BRAMPTON BRICK - BONNEVILLE STONE - POLAR WHITE - SEE SPEC. DRAWING 10-12-2020
- MASONRY - BRAMPTON BRICK - CONTEMPORARY SERIES - SHAKA 50 - SEE SPEC. DRAWING 10-12-2020. TYPICAL LAYOUT IS SEE STRIPPER.
- CEMENT PANEL SIDING - LAYTON & SHANK
- EXTERIOR INSULATION FINISH SYSTEM (EIFS) - DRYVIT - STONE GRAY # 11 - UNAPPROVED FINISH OR APPROVED EQUAL.
- EXTERIOR INSULATION FINISH SYSTEM (EIFS) - DRYVIT - STONE GRAY # 11 - UNAPPROVED FINISH OR APPROVED EQUAL.
- PRECAST CONCRETE TRIM - UNAPPROVED FINISH OR APPROVED EQUAL.
- CEMENT PANEL SIDING - WIDE BOARD SIDING - TIMBER BARK - 4x PANEL
- PREFINISHED WINDOW TRIM - BLACK ANODIZED SEE SPECIFICATIONS.
- COMPOSITE WOOD DECK - TREX - TRANSCENDS - 12x TORCH - 2x 15/4
- VENTED ALUMINUM SOFFIT - GENTEX - 3-PANEL 1' SOFFIT - BRIGHT WHITE OR APPROVED EQUAL.



2 REAR ELEVATION
A115 1:125



1 FRONT ELEVATION
A115 1:125

4.1.2 Townhouses

The proposed townhouses are 3 storeys in height (8.53 metres), situated on the eastern portion of the Subject Site. Blocks 1 - 3 contain 9 units per block and Blocks 4 - 6 contain 10 units per block. These blocks are arranged perpendicular to the modified Collector Road. Blocks 7 - 10, contain 6 to 8 units per block and are located side-by-side in a linear pattern and parallel to the southern property line. The placement of townhouse units on lots will support the objectives of street orientation and provides paired driveways, where feasible to minimize curb cuts and consolidate landscaped areas between driveways. Each townhouse unit features a minimum front yard setback of 6 metres from the road and a minimum rear yard setback of 7 metres for private outdoor amenity space. These units have integrated garages and driveways for parking facing the private condominium roads.

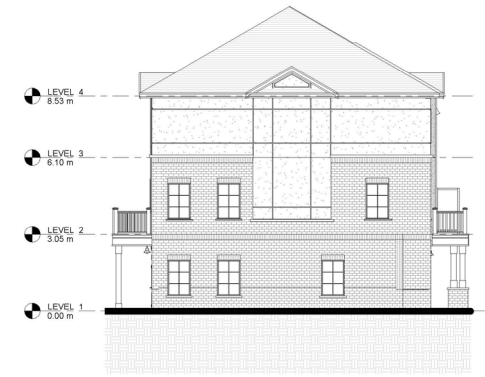
Blocks 1 – 3 have a minimum side yard setback of 7.2 metres and Blocks 4-6 have a minimum setback of 1.8 metres from the modified Collector Road limit, providing sufficient space for vegetative plantings to soften the urban condition and create a pedestrian-friendly experience.

4.1.2.1 Building Articulation and Materials

The building elevations display an attractive building design with contemporary finishing of brick and siding materials. Brick is used as the principal material and the upper storeys are treated with concrete cladding interjected with lintels and trim around windows and doors for accent and interest. Peaked roof pitches atop the latter distinguish the roofline. There are no blank facades; all elevations are detailed with glazing and articulation. Windows and doors are composed with regular intervals along the length of the building and sides to the extent possible. Distinguished horizontal lines are proposed throughout the building elevations to provide a vertical distinction between the storeys. Other architectural articulations include recessed unit entrance doors and paired balconies for upper storey with integrated railing system.



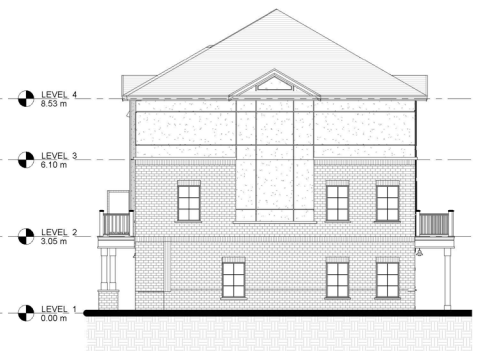
1 BLOCK 1 - FRONT ELEVATION
1/101A 1:100



3 BLOCK 1 - LEFT ELEVATION
1/101A 1:100

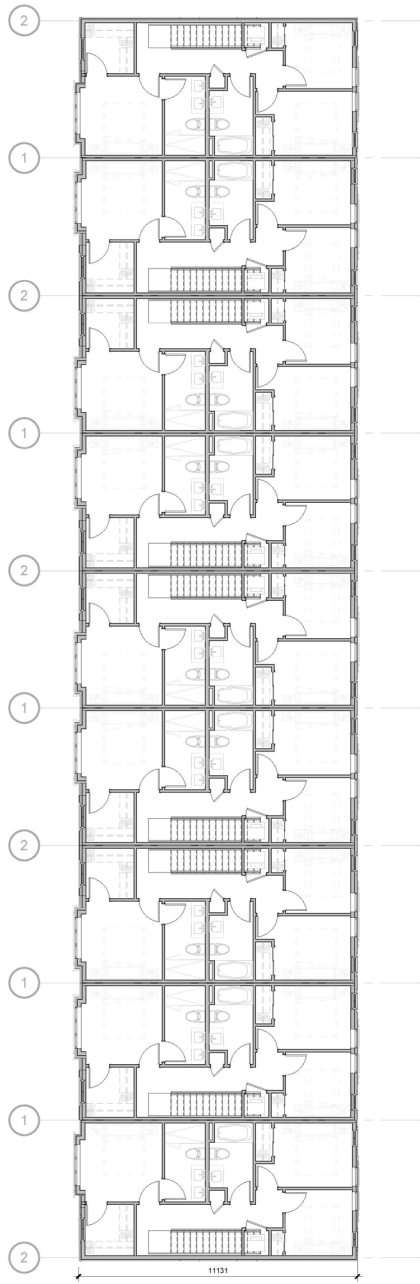


2 BLOCK 1 - REAR ELEVATION
1/101A 1:100

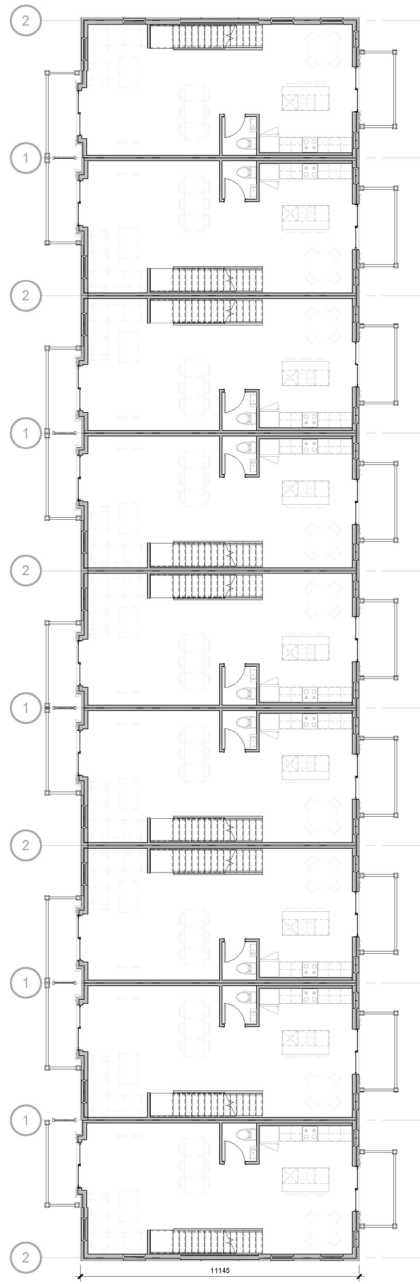


4 BLOCK 1 - RIGHT ELEVATION
1/101A 1:100

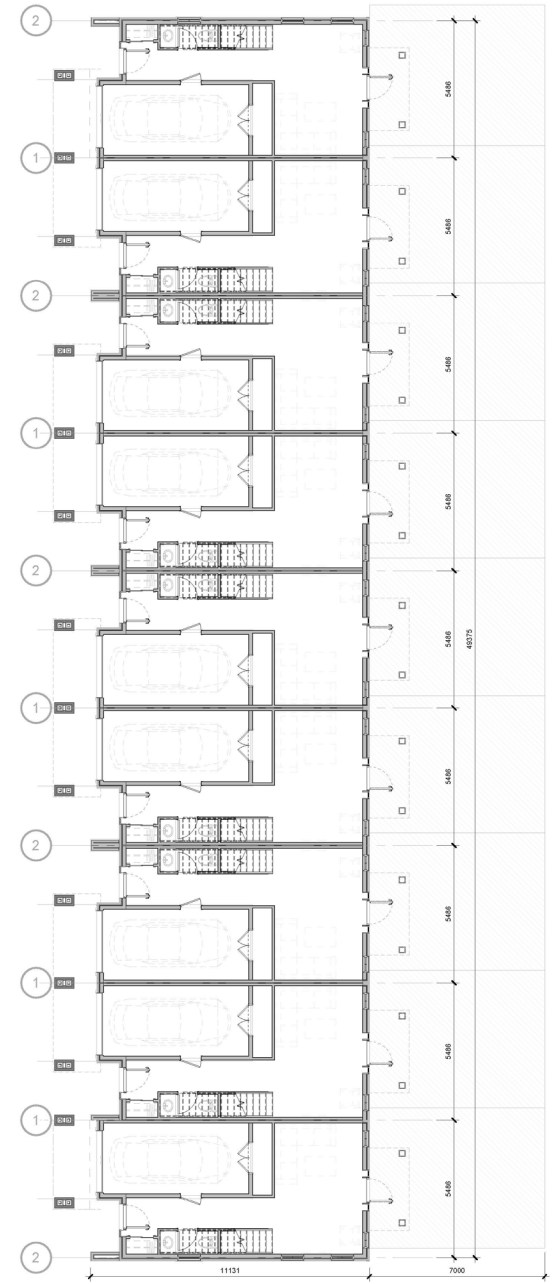
Townhouse Block Elevations, prepared by Chamberlain Architect Ltd. (January, 2023)



3 BLOCK 1 - LEVEL 3
A101/ 1:100



2 BLOCK 1 - LEVEL 2
A101/ 1:100



1 BLOCK 1 - LEVEL 1
A101/ 1:100

4.3 Access and Circulation

Two access/egress points to the developed portion of the Subject Site are from the modified Collector Road with connections to 10th Street East and 18th Avenue East, and 20th Avenue East. The private condominium roads accessed from the modified Collector Road are organized with a connected pattern and appropriately scaled development blocks. This configuration provides appropriate connectivity and ease of mobility, supports the delivery of emergency vehicles, and allows for future connections. The private condominium roads are proposed to be 7.6 to 7.8 metres wide, sized to allow sufficient space for traffic in both directions.

Sidewalks have been provided throughout, providing direct access to individual units, the common amenity area, surface parking area and the main east-west 20.5 metres road. All sidewalks along the private condominium roads are 1.8 metres wide and the sidewalk abutting the south side of the modified Collector Road is 3 metres wide. Where there is a crossing, they will be clearly delineated with demarcated crossings in an alternative material.

4.4 Garbage and Loading Access

Garbage storage/collection areas for each of the proposed apartment buildings will be located exterior to the building in an enclosure, screened from view from the public realm. A separate loading area is also proposed for each apartment building.

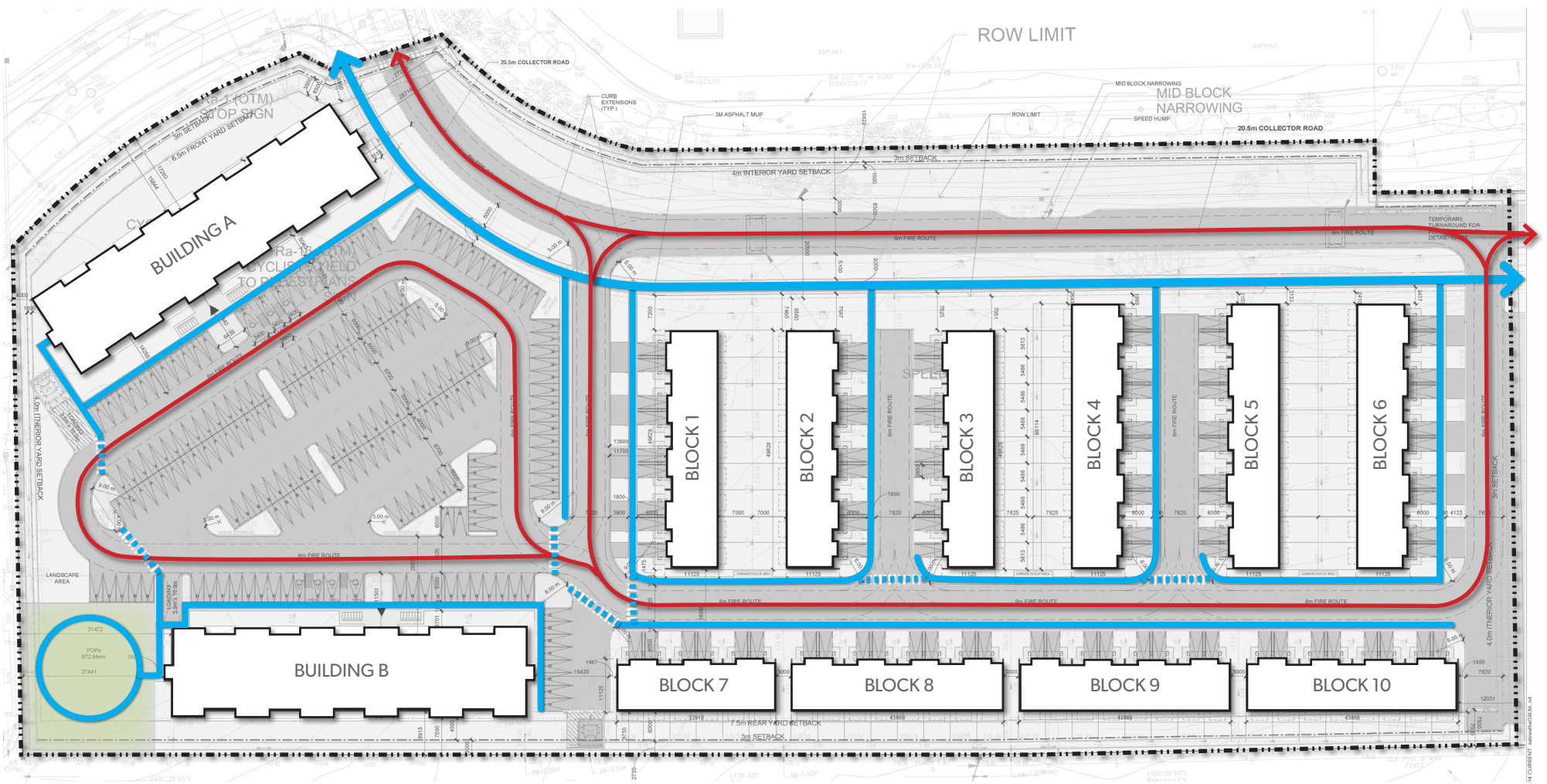
4.5 Parking

A total of 195 surface parking spaces are provided, including 7 barrier-free spaces for the apartment buildings. 14 bicycle racks will be provided near each of the building entrances for convenience use by visitors.

Each townhouse units consist of two parking spaces (1 garage, 1 driveway). The size and layout of garages and driveways will be based on the Zoning By-law requirements. The proposed driveways for the townhouse blocks are paired where possible to minimize curb cuts and maximize the area for front yard landscaping.

4.6 Lighting and Signage

Site lighting will be provided to ensure adequate wayfinding and security. A unified and coordinated approach will be taken for signage with an emphasis on architecturally integrated building signage. Signs will be designed not to obstruct pedestrians, other non-vehicular travel, and barrier-free movements. The design and placement of the signages will be further established through the detailed design stage.



- Vehicular Circulation
- Pedestrian Circulation

Pedestrian and Vehicular Connections for Phase 1 of the Proposed Development

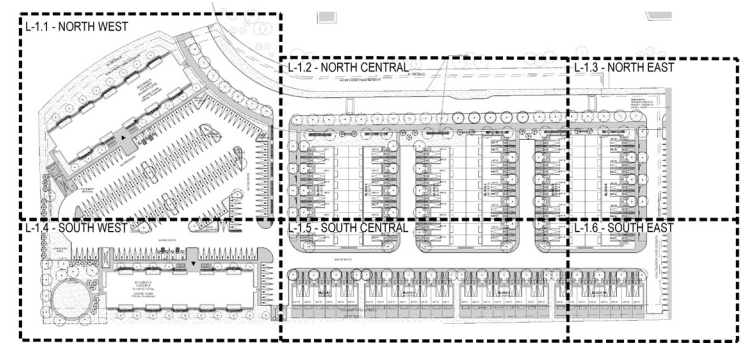
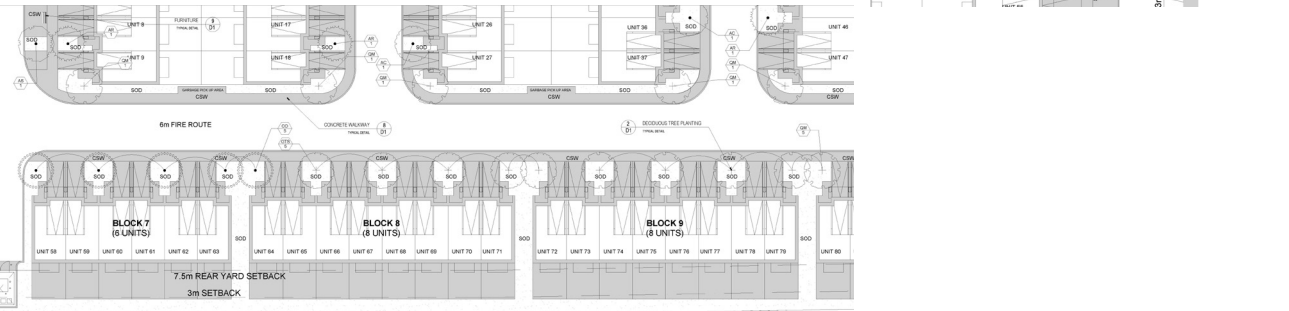
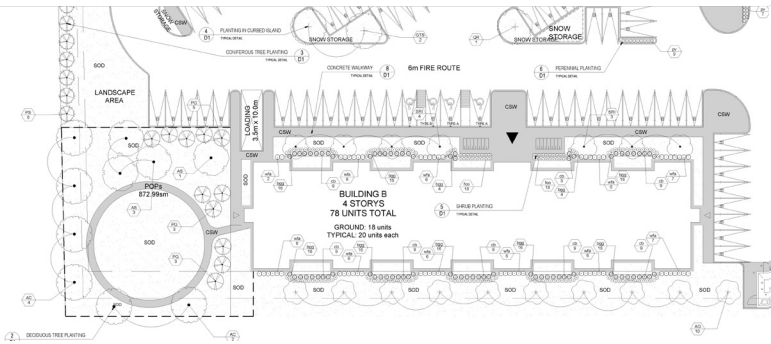
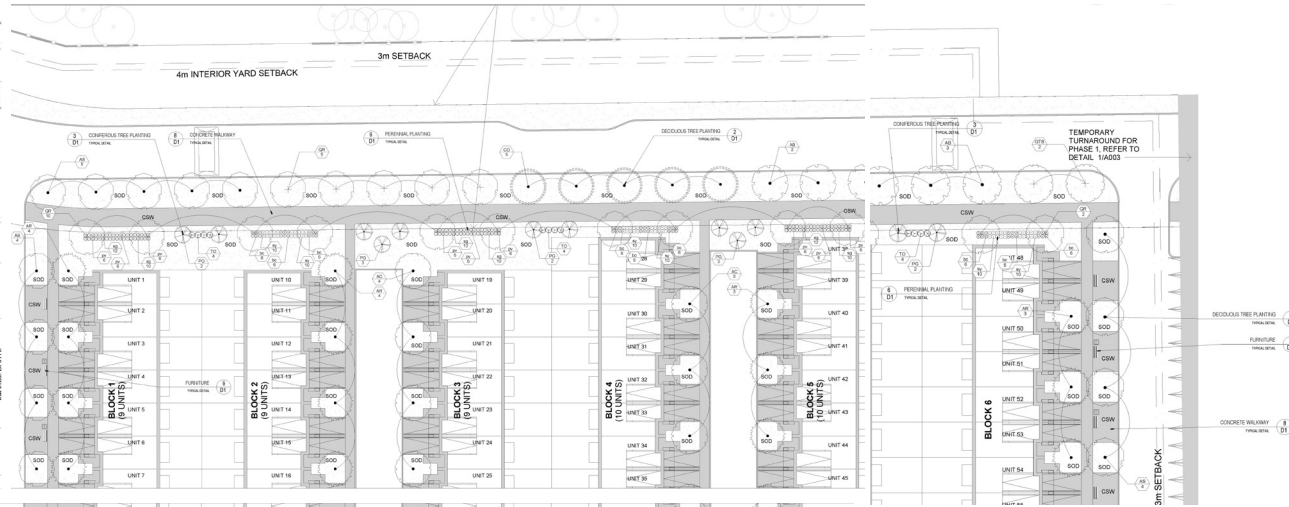
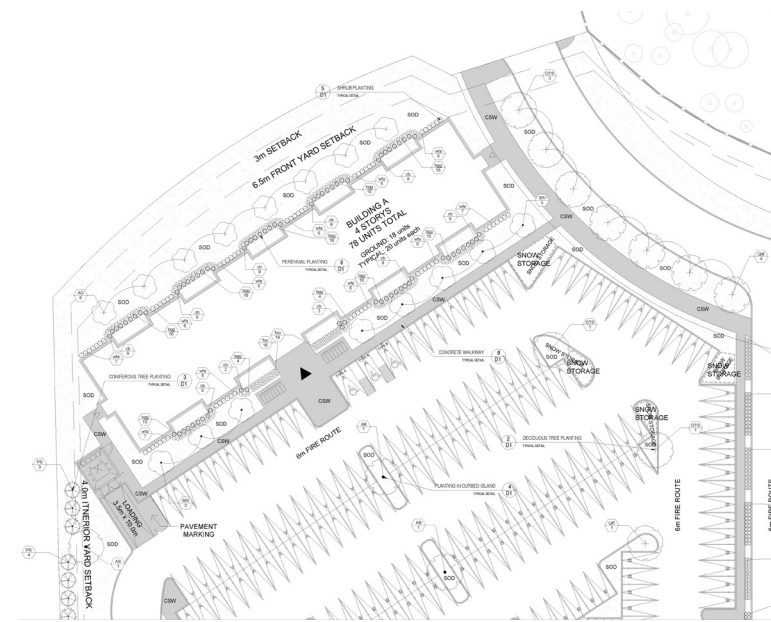
4.7 Landscape and Amenity Area

A landscape concept plan is prepared by MHBC and it is intended that the landscaping for the Subject Site will be used to enhance the public realm and create an attractive setting for future residents. The proposed Landscape Plan includes the following design elements:

- Landscaped front yard spaces between townhouse driveways and leading to unit entrances, composed of sodded areas with small deciduous trees (depending on utility conditions in those areas).
- Upgraded landscaped edges are proposed in the side yards abutting the modified Collector Road which includes generous planting zones and a 3-metre-wide sidewalk abutting the townhouse blocks. The landscaped areas will include coniferous and deciduous trees and understory perennial beds.
- Tree plantings are proposed in the side yards and throughout the surface parking lot of the apartment block. Landscaped islands will break up the parking spaces and reinforce the pedestrian walkways.
- Landscaped edges are proposed around the apartment buildings with sodded lawns, tree plantings and perennial beds to delineate the main buildings entrances as well as to provide privacy for the at-grade units.
- The private outdoor amenity area for the apartment block will include tree plantings for shade, paved pedestrian walkways, seating area and open lawn/free play area.
- Tree plantings will provide shade for amenity areas and contribute to softening the interface along property lines.
- Detailed design stage will include lighting plans and will provide sufficient illumination of the roadway and the pedestrian sidewalk.

The landscape materials proposed for the development have been selected based on their suitability for the site locations proposed. The selection of plants offers a variety of form and colour that will offer aesthetic interest to the Subject Site year-round, while framing the main entrance of the building, or screening of at-grade patios. The trees and understory

plantings proposed throughout the parking lot, front yards and at the main entrance to the apartment buildings are those which demonstrate higher tolerances to salt exposure and snow loading through winter maintenance and will require less frequent watering. This will result in a more sustainable landscape long-term by using less water and energy for maintenance and upkeep.



Landscape Plan, prepared by MHBC (January, 2023)

5. RESPONSE TO OFFICIAL PLAN POLICY

Section 8 of the Official Plan establishes the general urban design direction for new development in the City.

As outlined below, the applicable design policies of Section 8 generally speaks to both Phase 1 and Phase 2 of the development with the understanding that the detailed design elements of Phase 2 will be provided at a future site plan stage.

5.1 Natural Features (Section 8.2)

Access and Protection

8.2.1.5 - The protection and replacement of mature trees throughout the City is encouraged, particularly on the slopes of the escarpment and along the streets.

8.2.1.6 - Where new development is proposed, locations of existing mature trees should be considered in the preparation of a site plan. Through the implementation of the Tree Preservation Policy, the City will ensure tree protection in maintaining healthy trees. The City will require the planting of street trees in new development areas through a tree survey planting plan.

Response:

A detailed Tree Management Plan (TMP) for Phase 1 was prepared by Kuntz Forestry Consulting Inc. The majority of the deciduous forest stand area is dead and/or hazardous conditions and the removal of 76 trees is required to accommodate the Proposed Development and the remaining 30 trees can be retained with appropriate tree protection measures prior to the development. A separate TMP for Phase 2 will be prepared during the site plan stage.

Several new trees and landscaping will be introduced to the Subject Site. New plantings will include landscaping between the building facades and the streets that is complementary to the streetscape design, including sodded lawns and regularly spaced street trees.

5.2 Experiencing the City (Section 8.3)

Objectives

8.3.1.1 It is the City's intention to strengthen the positive image of Owen Sound as an attractive and liveable City by making the impression on entering and the experience of passing through the main streets enjoyable and easily understood.

8.3.1.3 The City will consider higher standards for permitted uses and the design of buildings and streetscapes along the main arterial streets. Design guidelines should be developed for uses fronting on these arterial streets, with appropriate building setbacks and points of access.

Response:

Building A, located near the entrance of the Subject Site, is sited parallel to the bounding public street with a generous front yard setback which maintains an appropriate street width to building height proportion that relates to pedestrian scale. The long edge of the building is articulated and establish a rhythm along the street edges with balconies, regularly spaced windows and doors, subtle depth cues and variation of building materials.

A high quality landscape design is maintained throughout the Subject Site with higher design standards applied along the modified Collector Road to enhance the visual aesthetics of the development. The upgraded landscaped edges proposed in the side yards abutting the south side of the modified Collector Road includes generous planting zones to screen the parking lot and for residential privacy, and a 3-metre-wide sidewalk. The landscaped areas will include coniferous and deciduous trees and understory perennial beds.

5.3 Pedestrian Environment (Section 8.4)

Objective

8.4.1.1 - The City will seek to secure a safe and accessible pedestrian environment in public and private development through the development and use of design guidelines which provide standards for clear visibility in public places, appropriate lighting, safe movement around vehicles, barrier free sidewalks and building entrances and such other measures as may be appropriate.

Living Streets

8.4.2.1 - Promote the objective of “Healthy Communities” through program and facility development. New neighbourhood streets should be designed to limit the speed of traffic and to create a continuous pattern of pedestrian connections, consistent with the principles of Living Streets. Sidewalks should, where possible, be separated from the roadway by landscaped boulevards.

8.4.2.2 - The City will promote the principles of “Living Streets” by:

- a. Encouraging walking throughout the City.
- b. Providing continuous safe and comfortable walking routes.
- c. Reducing potential pedestrian conflict with vehicles and separating pedestrian paths from roadways where possible.
- d. Reducing vehicular speeds on local roads.

8.4.2.4 - The City will promote public and private development that provides a comfortable, human scale environment, supports social interaction and which addresses the issues of year round use.

8.4.2.7 - The City will promote public and private development that achieves the optimum conditions of sun, shade and wind conditions throughout the seasons in all outdoor pedestrian areas. Conditions should be suitable for the expected activities, whether active, such as walking or passive, such

as sitting. Optimum conditions may be achieved through the appropriate siting of buildings and open spaces, the shape of taller buildings, the use of building elements such as awnings and walls and the use of landscaping elements to provide windbreaks and shade. In areas where taller buildings may be built or where there is direct exposure to open water, the City may require wind testing and shadow studies of proposed development and the adjacent pedestrian areas and public spaces.

Response:

A 3-metre-wide pedestrian sidewalk is proposed across the entire stretch of the site (both Phase 1 and Phase 2) on the south side of the modified Collector Road to provide east-west connection from the 10th Street East public sidewalk to 20th Street East. The design of the modified Collector Road also includes traffic calming measures to promote reduced vehicular speeds and minimize conflicts between road users. A 1.8-metre-wide internal sidewalk is proposed throughout the developed portion of the site.

The proposed road layout is designed to maximize convenience for mobility, ensure high-quality pedestrian environments and support emergency service vehicles. The clear visibility in the public area, appropriate lighting, and safe movement around vehicles, barrier-free sidewalks and building entrances will be appropriately provided.

The proposed configuration of the apartment buildings and townhouse blocks for both Phase 1 and Phase 2 provides an acceptable height and density transition and receives optimum conditions of sun, shade and wind conditions throughout the seasons in all outdoor pedestrian areas. At four storeys in height, apartment buildings remain human in scale, and in conjunction with the large lot area, and do not overwhelm the property.

5.4 Streetscape (Section 8.6)

Objectives

8.6.1.3 - The City may define, within any development area, building heights and setbacks or, in some cases, 'build to' lines that are consistent with the intended form of development and relevant guidelines. Within a residential area, such heights and setbacks should be consistent with the general form of existing development. Increased setbacks may be required where high buildings are proposed.

8.6.1.4 - The City may provide, or may require of new development, enhancement of the public right of way consistent with the character of the neighbourhood and applicable urban design guidelines, using streetscaping elements such as special lighting, landscaping, paving stones, street furniture, public art and other complimentary features and fixtures.

8.6.1.5 - Streetscape adjacent to parks, open space, natural areas or waterfront should be designed to visually extend the natural elements and spaces

Response:

The proposed built form and massing are appropriate and would fit within the existing and emerging context in the surrounding area and are generally consistent with the vision for the Sydenham Heights Planning area. Appropriate setbacks from the street edges and parking areas are proposed to provide sufficient room around the base of the buildings to soften the apparent mass with vegetative plantings.

The right-of-way will contain concrete pedestrian sidewalks, a landscaped boulevard between the sidewalk and curb with regularly spaced street trees and understory perennial plantings. Street lighting will be provided for sufficient illumination of the roadway and pedestrian sidewalk. Traffic calming options are also considered within the modified Collector Road to slow down speeds and promote a pedestrian-friendly environment.

The outdoor common amenity space is connected by the pedestrian sidewalks interior to the Subject Site, situated between Buildings A and B, and surface parking area. Glazing/windows along the elevations of the Buildings A and B promote natural surveillance opportunities to the outdoor amenity area.

Large Lots

8.6.5.3 - Consideration should be given to continuous pedestrian access along the building fronts near the street and connecting to adjacent development. Safe, comfortable, accessible and barrier pedestrian points of access from the street should be provided at regular intervals to all buildings on the lot.

8.6.5.4 - Connect City sidewalks to main, accessible entrances.

8.6.5.5 - In this form of development, parking shall be separated from sidewalks by a landscaped buffer. Sidewalks may also be set back from the road with landscaped boulevards where appropriate. These areas should be landscaped in accordance with applicable design guidelines.

Response:

The proposed private condominium road network will create desirable pedestrian and vehicular connectivity throughout the Subject Site, providing an interconnected and easily navigable road network with high permeability for safe and comfortable pedestrian access from the roads to individual townhouse units and to the main accessible entrances to the apartment buildings.

The sidewalks along the private condominium roads connect to the sidewalk along the modified Collector Road and to the public sidewalk along 10th Street East.

The landscaped areas between the proposed townhouse units provide sufficient space for front yard landscape treatments that contribute to an attractive pedestrian-oriented streetscape. The surface parking area for the proposed apartment buildings is separated from sidewalks by landscaped buffers. Plantings are

provided throughout the proposed surface parking area, including landscaped islands and walkway connections, where feasible to break up the visual extend of the parking areas.

Certain portions of the townhouse block where appropriate maintains sidewalk setback from the road with landscaped boulevards. For the remainder of the townhouse block where sidewalks are abutting the roads consists of paired driveways to minimize curb cuts and consolidate landscaped areas between driveways.

Parking and Access

8.6.6.1 - Where off street parking is required, the City may develop and enforce design criteria and guidelines to provide for:

- a. Safe vehicular access.
- b. Pedestrian safety, convenience and accessibility.
- c. Adequate lighting, signage and landscaping.
- d. Controlled visual impact by appropriate location on the site.

8.6.6.2 - Parking should be located in a manner appropriate to the size of the site, the optimum relationship of the building to the street and convenient access for users as described in Section 8.6.

8.6.6.3 - Access points should be clearly visible and distinguishable, limited in number and designed in a manner that will minimize hazards to pedestrian and motor traffic in the immediate area. The City may require consolidation of adjacent parking areas to provide appropriate spacing of access points on arterial roads.

8.6.6.4 - Pedestrians should have a safe, well-lit, clear route from vehicle to building at all times of the year. Safe, comfortable, accessible and barrier-free pedestrian points of access should be frequent and easily identified with clear directional signage. Larger developments should provide sitting areas for pedestrians awaiting pick-up.

8.6.6.5 - Parking areas are to be adequately landscaped in accordance with urban design guidelines. Parking areas should maintain distinct street edges through appropriate landscaping or structures.

Response:

The Subject Site has clearly defined points of access for pedestrians, and walkways to facilitate safe circulation. The private condominium roads and driveways through surface parking areas in the apartment block will be appropriately landscaped and provide distinct vehicle and pedestrian areas. Barrier free spaces are provided in accordance with the Zoning By-law. Planting strips and landscaped edges will frame the surface parking areas and driveways to minimize the aesthetic impact of the surface parking. The size and location of driveways for the townhouse blocks will comply with Zoning By-law requirements. Driveways and aprons will be paved using course asphalt or concrete paver materials. The laying out of driveways will consider optimizing opportunities for front yard landscaping through pairing of driveways.

Adjacent Uses

8.6.7.2 - The City shall consider the potential impact of abrupt changes in building height and scale on surrounding uses and protected views when considering zoning by-laws and site plan approvals. Generally, the City will seek compatible building forms.

8.6.7.3 - Where the height or mass of a proposed building may significantly shadow or cause increased wind conditions on an adjacent open space or solar collector, the City may require a study be undertaken to assess the impact and may develop and enforce setback requirements, site plan requirements and design guidelines to ameliorate the problem.

Response:

The configuration of the Subject Site, including the modified Collector Road and private condominium roads and the proposed mix of housing types are compatible building forms and ensures adequate physical separation from the surrounding existing uses and proposed within and outside the property. Shadow Study is not required for the four-storey apartment buildings.

New Residential Development

8.6.8.2 - Buildings should be sited to provide relatively consistent streetscapes within an area, with similar setbacks, defining the visual width of the streets. Street trees should be planted in accordance with an appropriate plan.

8.6.8.3 - In new comprehensively designed residential development, the City may permit building setbacks and lot sizes which vary from surrounding development where these are determined to be appropriate to the proposed form of development in accordance with accepted design guidelines.

Response:

The Proposed Development provides a consistent building setback and maintains an appropriate street width to building height proportion that relates to pedestrian scale. The Proposed Development represents an appropriate design response to the site's emerging context and provides variety of built forms that have regard to the adjacent existing and planned residential developments

Outdoor Lighting

8.6.9.1 - New development and redevelopment shall be designed with responsible lighting practices that create safe outdoor environments and minimize glare and impact to night sky, public view and surrounding properties.

Response: Lighting for the Subject Site will be considered at the detailed design stage but will ensure on-site lighting provides appropriate illumination of exterior areas while minimizing light trespass to surrounding properties.

Landscape Design

8.6.10.1 - Landscaping shall be designed to enhance the presence of each building and used as a major visual element to unify the proposed building, streetscape and surrounding environment.

8.6.10.2 - Landscaping design shall utilize a diversity of plant material that are appropriate for the site conditions in all seasons, including a focus on native plant material where appropriate, and avoid the use of invasive species in accordance with City policies and guidelines.

Response:

A detailed landscape design has been prepared by MHBC for Phase 1 of the Proposed Development. The landscape plan incorporates landscaping between the building facades and the streets that is complementary to the streetscape design, including sodded lawns and tree plantings. Tree plantings and massed understory plantings will be proposed between the buildings and throughout the surface parking lot to break up the surface parking area and reinforce the pedestrian walkways. Tree plantings will provide shade for amenity areas and contribute to softening the interface along property lines. Peripheral landscaping will be included to frame the bounding streets, soften the edges of buildings, and screen undesirable views such as utility equipment. Water efficient and drought resistant landscaping will be considered, including using native planting materials.

5.5 Safety and Security (Section 8.7)

Public Safety in the Built Form

8.7.1.1 - New development and redevelopment shall be designed to promote safe, comfortable and accessible environments for all users.

8.7.1.3 - Physical design measures such as adequate lighting and clear sightlines should be used as basic design elements.

Response:

The site will be well lit and clearly defined. There will be no blank facades; all building elevations will be highly articulated and will have regularly spaced openings, to ensure "eyes on the street" and provides visibility between building interiors and parking areas/open areas. Adequate lighting will be provided for parking areas and pedestrian areas, to enhance the sense of safety and illuminate various areas throughout the Subject Site.

6. SUMMARY

The application is seeking for Official Plan Amendment and Zoning By-law Amendment and Site Plan Approval applications to facilitate the development of residential uses, as shown in the Overall Site Plan (Page 5). The numerous design considerations described within this Urban Design Brief demonstrate that the Proposed Development conforms to the City of Owen Sound's Urban Design policies of its Official Plan. The design objectives for the Subject Site will create a compatible residential development that is functional, accessible, attractive, safe, sustainable, and cost-effective.

Thoughtful consideration of the organization of the site layout in terms of circulation, interfaces, linkages, mix of building typologies and landscaping will support quality public spaces. The design will incorporate high-quality architecture and landscaping and consider how the individual elements support the character of the community as a whole. The urban design attributes of the Subject Site will reflect principles of good urban design and fit sensitively with the character of the City of Owen Sound.